

SITE PLAN

EXISTING METRO TRANSIT STOP

PRIMARY ENTRANCE

VISITOR BIKE PARKING
SIGN (ON BUILDING)

BIKE ROUTE

RAIN GARDEN WITH NATIVE
PLANTINGS

POLLINATOR LAWN EDGE

TREE, SHRUBS AND GROUND
COVER PLANTINGS

6' PERIMETER FENCE

RESIDENT PATIO

TREE, SHRUBS AND GROUND
COVER PLANTINGS

SEAT WALL

SHADE STRUCTURE

BACK ENTRANCE GATE

TRANSFORMER WITH
SCREENING FENCE

6' PERIMETER FENCE

PROPOSED BUILDING

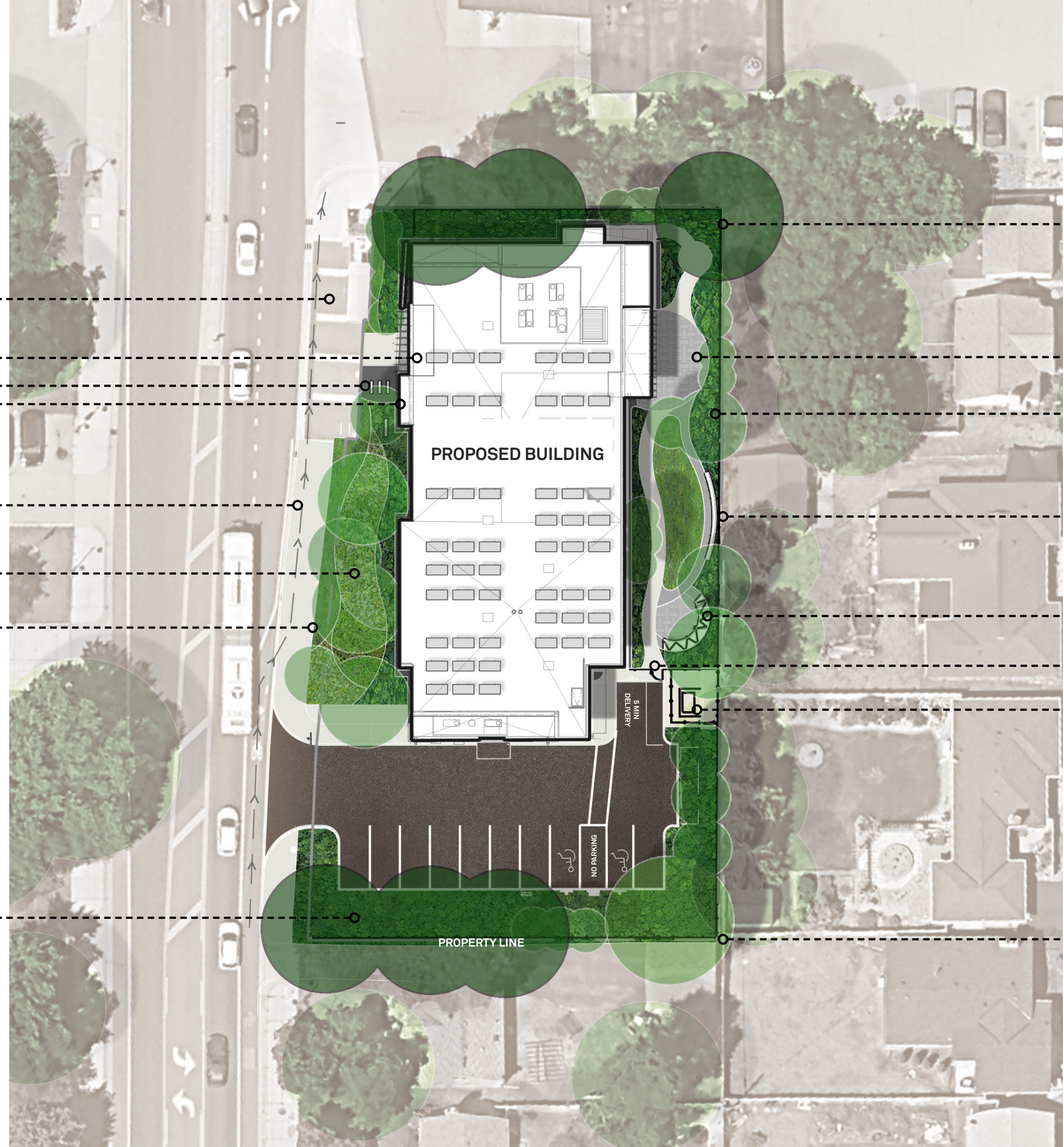
ADMIN
DELIVERY

NO PARKING

PROPERTY LINE

EXISTING TREE TO REMAIN

PROPOSED TREE



Project No. 2022018

Aster Commons

6613 Portland Avenue S.
Richfield, MN 55423

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the Laws of the State of Minnesota.

Architect Seal

Signature _____
Print Name _____
Date _____ License No _____

Design Development

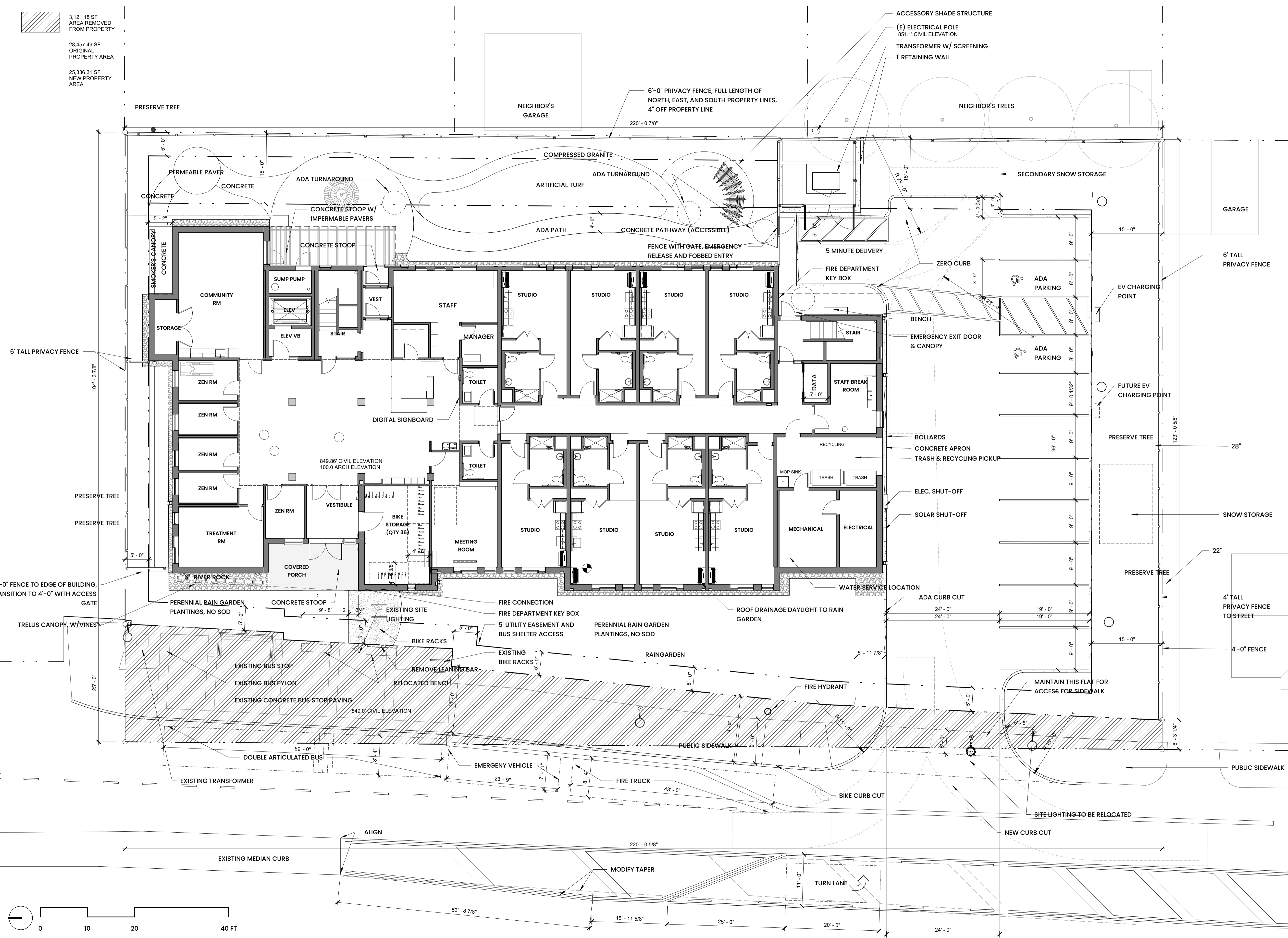
ISSUE / REVISION

Mark	Date	Description
	2023.08.10	CITY REVIEW COMMENTS

NOT FOR CONSTRUCTION

SITE PLAN

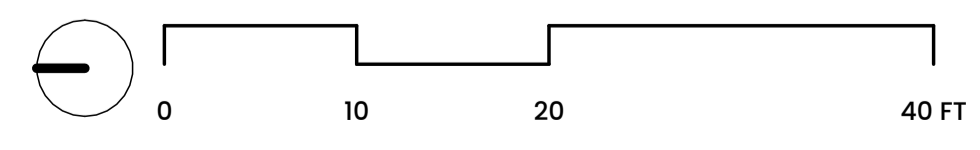
A001



3,121.18 SF
AREA REMOVED
FROM PROPERTY

28,457.49 SF
ORIGINAL
PROPERTY AREA

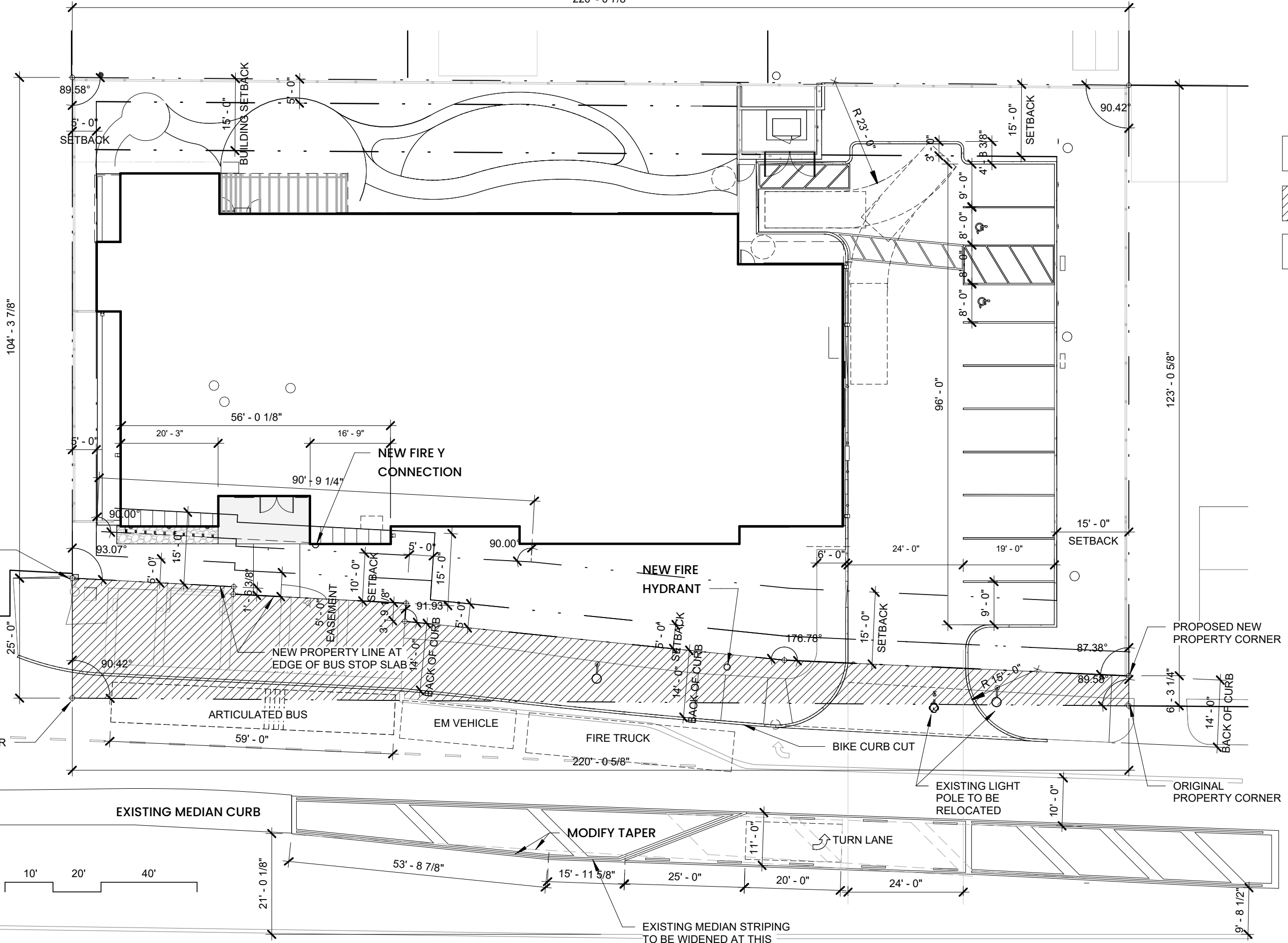
25,336.31 SF
NEW PROPERTY
AREA



1 PRES SITE PLAN
A001 1" = 10'-0"

Drawing 2022 Copyright: Meyer, Scherer & Robinson, LLC 18112023A-0061819-100 ©/Project/2023018_A001_Pres_Site_Plan

220'-0 7/8"



- 28,457.49 SF ORIGINAL PROPERTY AREA
- 3,121.18 SF AREA REMOVED FROM PROPERTY
- 25,336.31 SF NEW PROPERTY AREA

PROPOSED NEW PROPERTY CORNER

ORIGINAL PROPERTY CORNER

PROPOSED NEW PROPERTY CORNER

ORIGINAL PROPERTY CORNER

EXISTING MEDIAN CURB

MODIFY TAPER

TURN LANE

EXISTING MEDIAN STRIPING TO BE WIDENED AT THIS POINT FOR TURN LANE

EXISTING LIGHT POLE TO BE RELOCATED

1 SITE PLAN PROPERTY BOUNDARIES

AP001 1" = 20'-0"



1 BUILDING WEST ELEVATION - SHADED

AP211 1/16" = 1'-0"



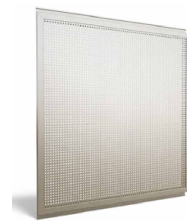
BRICK - ENDICOTT MANGANESE IRONSPOT, VELOUR FINISH, NORWEGIAN SIZE



SIDING 1: NICHHA LATTURA V-GROOVE, WHITE



SIDING 2: NICHHA VINTAGEWOOD, SPRUCE



MECHANICAL SCREENING: CITYSCAPES ENVISOR PERFORATED PAN

MATERIAL

- BRICK
- NICHHA WHITE LATTURA V-GROOVE
- NICHHA SPRUCE VINTAGEWOOD
- WINDOWS+ GLASS DOORS
- METALS (TRIM, LOUVERS, PANELS, SCREENS)

SQUARE FOOTAGE

- 1321
- 1879
- 771
- 1289
- 160

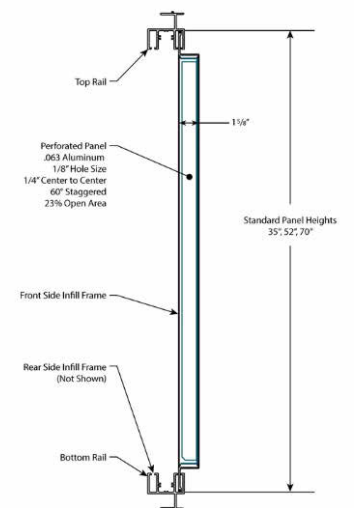
% of FACADE

- 24%
- 34%
- 14%
- 24%
- 3%

5,419 sf

PERFORATED METAL SCREEN CROSS SECTION

PAN PERFORATED METAL CROSS-SECTION



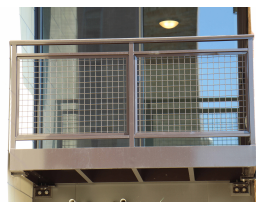


1 BUILDING NORTH ELEVATION - SHADED

AP212 1/8" = 1'-0"



BRICK - ENDICOTT MANGANESE IRONSPOT, VELOUR FINISH, NORWEGIAN SIZE



SIDING 1: NICHIIA LATTURA V-GROOVE, WHITE



SIDING 2: NICHIIA VINTAGEWOOD, SPRUCE



MECHANICAL SCREENING: CITYSCAPES ENVISOR PERFORATED PAN

BALCONY: MIDWEST IRON METAL HANDRAIL BALCONY

MATERIAL

- BRICK
- NICHIIA WHITE LATTURA V-GROOVE
- NICHIIA SPRUCE VINTAGEWOOD
- WINDOWS+ GLASS DOORS
- METALS (TRIM, LOUVERS, PANELS, SCREENS)

SQUARE FOOTAGE

- 688
- 548
- 672
- 579
- 99

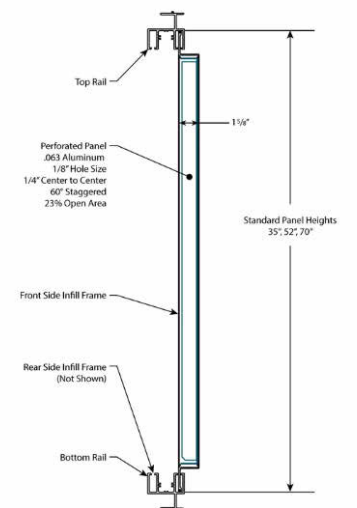
2,586 sf

% of FACADE

- 27%
- 21%
- 26%
- 22%
- 4%

PERFORATED METAL SCREEN CROSS SECTION

PAN PERFORATED METAL CROSS-SECTION





NOTE: SEE WEST ELEVATION FOR BUILDING HEIGHT AND MATERIAL DESIGNATIONS

1 BUILDING EAST ELEVATION - SHADED

AP213 1/16" = 1'-0"



BRICK - ENDICOTT MANGANESE IRONSPOT, VELOUR FINISH, NORWEGIAN SIZE



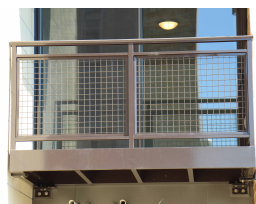
SIDING 1: NICHHA LATTURA V-GROOVE, WHITE



SIDING 2: NICHHA VINTAGEWOOD, SPRUCE



MECHANICAL SCREENING: CITYSCAPES ENVISOR PERFORATED PAN



BALCONY: MIDWEST IRON METAL HANDRAIL BALCONY

MATERIAL

BRICK	370	7%
NICHHA WHITE LATTURA V-GROOVE	1488	22%
NICHHA SPRUCE VINTAGEWOOD	1921	6%
WINDOWS+ GLASS DOORS	1253	23%
METALS (TRIM, LOUVERS, PANELS, SCREENS)	374	7%

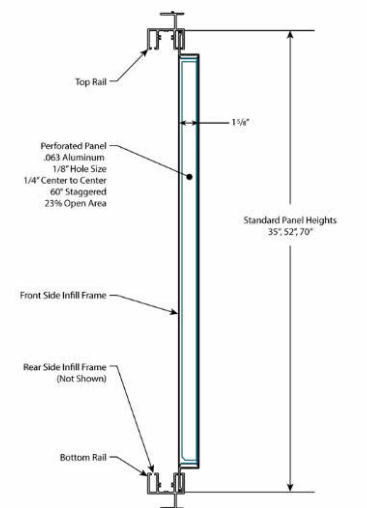
SQUARE FOOTAGE

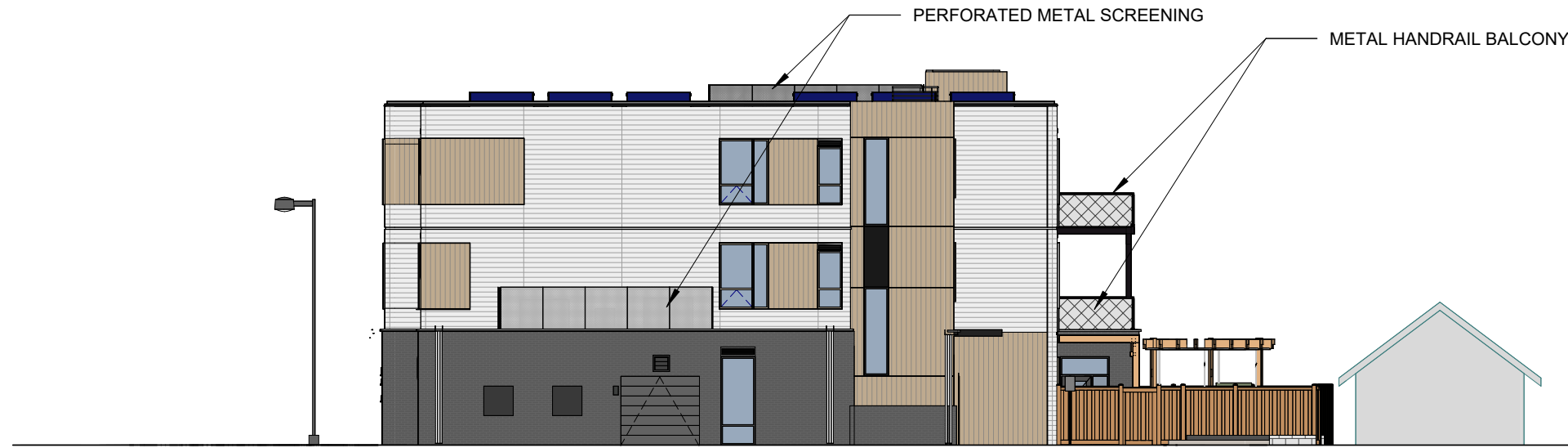
5,406 sf

% of FACADE

PERFORATED METAL SCREEN CROSS SECTION

PAN PERFORATED METAL CROSS-SECTION





NOTE: SEE WEST ELEVATION FOR BUILDING HEIGHT AND MATERIAL DESIGNATIONS

1 BUILDING SOUTH ELEVATION - SHADED

AP214 1/16" = 1'-0"



BRICK - ENDICOTT MANGANESE IRONSPOT, VELOUR FINISH, NORWEGIAN SIZE



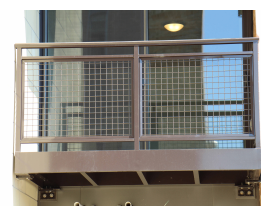
SIDING 1: NICHIIA LATTURA V-GROOVE, WHITE



SIDING 2: NICHIIA VINTAGEWOOD, SPRUCE



MECHANICAL SCREENING: CITYSCAPES ENVISOR PERFORATED PAN

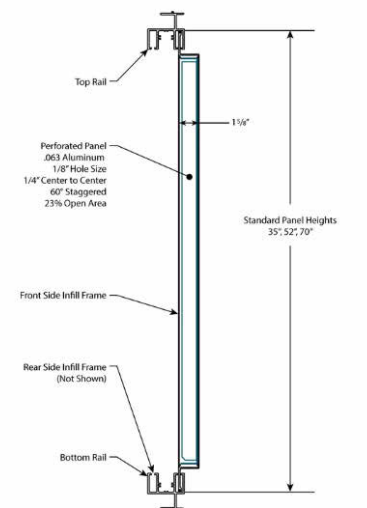


BALCONY: MIDWEST IRON METAL HANDRAIL BALCONY

MATERIAL	SQUARE FOOTAGE	% of FACADE
BRICK	1321	9%
NICHIIA WHITE LATTURA V-GROOVE	1974	39%
NICHIIA SPRUCE VINTAGEWOOD	676	24%
WINDOWS+ GLASS DOORS	187	8%
METALS (TRIM, LOUVERS, PANELS, SCREENS)	230	9%
	2,425 sf	

PERFORATED METAL SCREEN CROSS SECTION

PAN PERFORATED METAL CROSS-SECTION









PROJECT SUMMARY

Aster Commons

6613 Portland Avenue South

Richfield, MN 55423

Aster Commons is a new, 3-story building of wood construction and slab on grade with a flat roof housing solar panels, to be installed provided they are funded. When completed the building will provide 38 studio apartments targeting neurodiverse young adults. The backyard will be extensively landscaped (trees and shrubbery) with a 6' high fence abutting the neighbor's properties to provide a safe space for these vulnerable young adults. Parking is provided by 11 onsite parking stalls at the south end of the site. The building will have security cameras located in interior common spaces and around the building perimeter to protect the residents' safety. The building provides a variety of spaces to support residents' wellbeing including property management, case management, therapy and counseling services, community rooms, a 3-season porch, meeting rooms, and quiet rooms. Residents must comply with income limits of 30% of AMI for 50% of the apartments and 30%-50% AMI for the other 50% of the apartments. The building is 28,079sf and is predicted to have 3-4 staff onsite throughout the day.

MSR Design has discussed this proposal with city staff and believe the following items need to be addressed in this application:

- Comp Plan Amendment to Reguide from MDR to HDR
- Rezoning from MR-2 to MU-N
- Variance for a 11-stall parking lot
- Variance for a 10' setback for building location in the front yard

COMPREHENSIVE PLAN AMENDMENT

The project is requesting a Comprehensive Plan amendment as the current guiding only allows for 34 dwelling units per acre and Aster Commons would exceed that limit by 4 units.

The proposed Comprehensive Plan Amendment to re-guide from MDR to HDR is justifiable because this is a node within the city that has been identified to have a higher land use capacity than the surrounding single-family neighborhood. The current MDR designation does not support the ambitions of the Veterans Park Area Overlay (VPA) zoning district by its own definition in Section 541.25 Subd. 1: "...to produce structures of consistent character and of appropriate scale that transition from single family residential to higher density mixed use..." (italics added for emphasis).

This site is located along Portland Avenue, a primary north-south arterial in the city of Richfield that contains many large commercial and institutional buildings along its spine between highway 62 and interstate 494, including churches, schools, and the Richfield Municipal Complex one block south of this site. This is an ideal site for a reguiding to HDR to allow this site to provide higher density housing and to provide affordable housing that helps the City of Richfield meet the community's Met Council affordable housing goals.

The site is in close proximity to commercial offerings as well as located along the BRT and adjacent to the 66th Street bus routes and protected bike path. The area is highly walkable and the nearby Veterans Park is an ideal municipal amenity that should have density nearby as a natural support area for residents.

This proposed use is a useful transition/buffer from the lower density single-family neighborhood behind the property to Portland Avenue, continuing the densification along Portland Avenue. Thus we believe the proposed regarding to HDR is congruent with the intents of the VPA and the reality of this property's best and highest use along Portland Avenue.

REZONING REQUEST

Proposed to rezone from MR-2 to MU-N to enable this proposed project to conform with the MU-N zoning designation with two minor variances needed. The current MR-2 zoning does not support the ambitions of the Veterans Park Area Overlay (VPA) district, as written in the municipal zoning code Section 541.25 Subd. 1: "...to produce structures of consistent character and of appropriate scale that transition from single family residential to higher density mixed use..." (italics added for emphasis). By definition, the MR-2 zone is a lower density zone that does not support the VPA's intentions to create a node of vitality at and adjacent to the Portland Avenue and 66th Street roundabout that seeks to diversify housing options per municipal zoning code Section 541.25 Subd. 1: "...to expand the mix of housing in the area such as row/townhouse, affordable units, courtyard apartments, courtyard cottages, and live-work units."

Additionally, Richfield has committed to supporting the Met Council's goals of providing 121 units of affordable dwelling units at or below 80% of the area median income by 2030. This project will help the city meet its long-term commitment to build affordable housing for Richfield by adding 38 new affordable units at or below 50% of the area median income.

VARIANCE REQUESTS

PARKING REDUCTION VARIANCE

Under the current zoning, only 20 units would be allowed and that would mean the project would need 25 parking stalls at 1.25 stalls per unit.

Proposed parking is 11 total parking stalls = 0.29 parking stalls per dwelling unit.

(a). Is there a practical difficulty present which denies a reasonable use of the property? (Explain):

Yes, the additional parking would result in a much smaller building to allow the zoning required parking count to fit onsite and much of that parking would always sit empty. A parking study was conducted by Beacon Interfaith Collaborative through the winter of 2022/2023 to measure total parking demand at three similar buildings owned by Beacon. This was done at the suggestion of Richfield city staff when the issue of excessive parking requirements for the target resident population was discussed at the Sketch Plan review session in August 2022. The study found that at any time a maximum of 8, 6, and 7 parking stalls were actually used at the three properties, each of which is larger than the proposed Aster Commons and are targeted to more all-purpose young adult housing rather than Aster Commons'

targeted population of neurodiverse young adults. Thus we are convinced the demand at Aster Commons will not exist for residents and staff to need more than the 10 car parking stalls provided onsite.

An additional parking stall is being provided and will be signed for Metro Mobility and ride share vehicles to pick up/discharge residents at the parking lot entry to the building. Four exterior bike parking spots and an interior bike storage room accommodating 38 bicycles are in the proposed design, including indoor space for 2 adaptive bikes with electric plug-ins (See Sheet A193 for layout and dimensions). Residents at Beacon's three other properties are very frequently users of these modes of transit and we anticipate that to be true for this property. Metro Mobility, ride share, buses and bicycles are predicted to be the primary means of transport for this population since this population has a higher rate of physical impairments than the general population and their incomes restrict the funds required to own/maintain an automobile. Thus having the parking stall count required by the code would create a sea of parking lot on this property since underground parking is cost prohibitive. Additionally, many of the residents will be unable to secure a driver's license due to their neurodiverse medical conditions.

(b). Are there any unusual or unique circumstances relating to the property or building which are beyond your control? (Explain):

Yes, the current site conditions of the new BRT station's proximity to new roundabout reduce the lot size and force the curb cut to be at the south end of the property. Both remove available site square footage for parking and limit building placement.

The proposed use of Metro Mobility and the adjacent BRT station aligns with city goals for reducing traffic and congestion. Thus the proposed reduced parking count aligns with overall city goals to reduce automobile traffic. The location along the BRT line, public sidewalks, and bike lanes along Portland Avenue and 66th Street are optimal for this population's predicted needs for transportation.

We believe this parking reduction represents less site burden than other uses that would typically be permitted by zoning that would allow at least 24 parking stalls. Thus granting this variance so this project can go forward as proposed will actually reduce individual automobile traffic since residents will be primarily utilizing public transportation, walking, and bicycle options for the most part.

(c). Is the variance consistent with the purpose and intent of the rule from which a variance is being requested? (Explain):

Yes, the purpose of this zoning code section is to provide sufficient off-street parking for the sites intended use; in this case that is residents, staff, and guests. Based on the parking study conducted by Beacon Interfaith Collaborative (noted above) at the buildings they own and manage in Edina, Minneapolis, and St Paul the parking demand will be less than or equal to the 10 parking stalls proposed. The results of this study were sent to city staff by Beacon in January 2023.

Per current zoning allowed for the site, there could be 24 dwelling units built onsite. Parking could be provided in underground and/or surface parking. If these were 3-bedroom apartments, there could reasonably be 72 to 96 residents onsite (assuming families with 2 children). At the zoning code required mandate this would result in only 30 parking stalls, which would likely mean additional parking on the nearby side streets since parking is not allowed on Portland Avenue. Thus we believe our proposal is a less intensive use of the site than if the building constructed were to be maximized per the current

zoning allowances. And that our proposed parking reduction will decrease neighborhood impact by reducing the level of traffic on the site and entering/exiting the property.

FRONT YARD SETBACK VARIANCE

Required property front setbacks are 15' min to 25' max per zoning code section 537.07 Bulk and Dimensional Standards MU-N table 2.

Proposed front setbacks are from 10' minimum to 25' maximum.

Maintaining 15' minimum front set back is doable except at the BRT station because that station infringes on the efficient use of the site.

The original site consists of the following lots 31, 32, 33 and 34 for a total of 28,478 square feet or 0.654 acres. Front building setback responds to revised site conditions with the recent roundabout installation and BRT station installed by Metro transit. The current site has been reduced to 25,266 SF. The building follows the recommended 15' setback for main front yard setback except at BRT bus shelter where we are requesting a 10' setback from the BRT station since the station sits within the 5' area of the setback. Side yard setbacks are 5' on the north and east yards and a 15' south setback has been created for the parking lot as required for adjacency to residential property. The east rear yard setback has been increased to 27'-4" to accommodate private resident features including rear patio space, walking paths, private seating areas and landscape.**(a). Is there a practical difficulty present which denies a reasonable use of the property? (Explain):**

Current site conditions with the new BRT station have resulted in the property being narrower at its north end due to the BRT bus stop that steps back into the site by 5'. This step back has necessitated the building being 10' from the front property line, which sits at the rear edge of the 5' indent for the bus shelter, sign, and bench. If this step-back in the property line had not been present, the building position would comply with zoning and this variance would not be necessary.

The project is also maintaining a min of 27' for the rear yard area to maintain the min of 10% useable area and to maintain more distance from the neighbors who front on the street to the east. The building placement is aligned to avoid solar shading of neighbors per the zoning code.

(b). Are there any unusual or unique circumstances relating to the property or building which are beyond your control? (Explain):

After talking with existing neighbors who live behind the property, we believe the best building location is to push it to the west and as far from the rear property line as possible. The current site layout accomplishes this by including a 27' setback on the rear (east) side of the site to provide as much separation as possible between the building and neighbors. This allows the project to include taller trees to be planted between the new building and rear property line to help screening between the building and the neighbors. If the site were to meet the requirements of the 15' minimum setback at the north end of the site, it would be a very convoluted building to maintain the 25' maximum setback at the south end of the site since the site is trapezoidal in shape. We believe this design layout better situates the building on the site than strictly complying with the 15' minimum setback rule.

The remainder of the proposed building adheres to the 15' setback except at the entry area due to the removal of original site sf for the BRT shelter, signage and bench.

(c). Is the variance consistent with the purpose and intent of the rule from which a variance is being requested? (Explain):

The newly constructed BRT station sits within the prior, typical 15' setback requirement for the property. Had the original property line been maintained then the building placement would comply with the 15' minimum setback dimension. The 15' minimum setback is maintained south of the bump into the property for the BRT shelter, bench, and sign.

Aster Commons – Determination of Parking Need

2023

In developing the preliminary designs for the Aster Commons 38-unit supportive housing development, determining the need for parking has been carefully considered. The site is on the METRO D Line as a part of the growing network of bus rapid transit (BRT) lines that deliver a faster trip and an improved transit experience with service from Bloomington through Minneapolis to Brooklyn Center.

We believe the residents who will live at Aster Commons will be youth and young adults, some with neurodiverse conditions, emerging from homelessness without access to vehicles and heavily dependent on the metro transit system and bicycling thus requiring minimal parking. The site has a METRO D Line stop steps from the front door and ample bike storage will be provided.

To validate our theory of parking demand needs, we conducted a parking study at three of Beacon’s other youth/young adult buildings located in St. Paul (on the Green Line) and Minneapolis (on the 18 & 23 metro lines) and Edina . These buildings have similar populations, are all studio apartments and similar transit access.

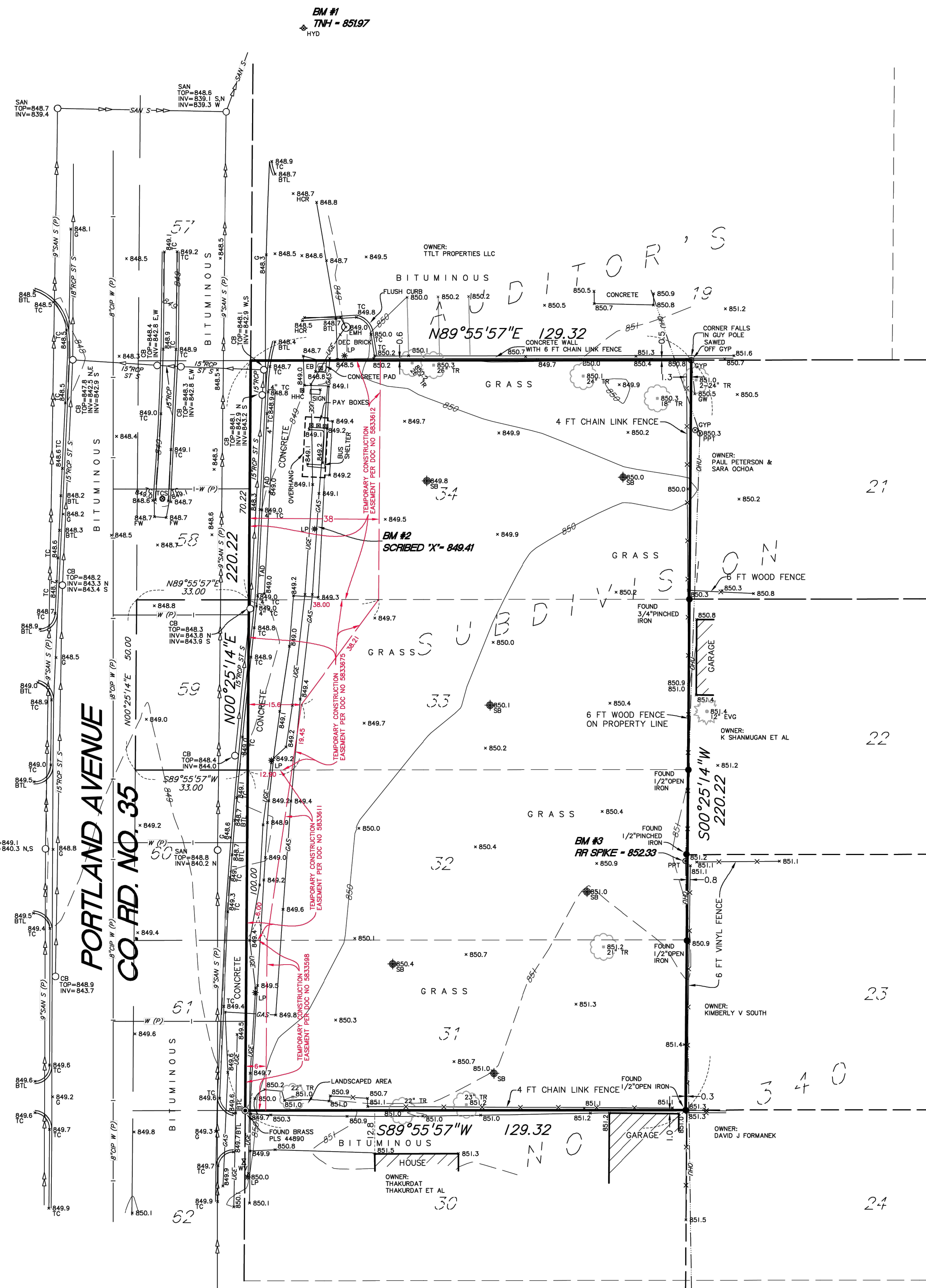
Based on our study, tracking parking activity for a month period, we’ve determined that the planned eleven parking spaces proposed for the Aster Commons development will be sufficient and we will seek necessary approvals to proceed in this direction. At no time during this study were the parking spaces full or over extended.

The Parking Study:

Prior Crossing		Nicollet Square		66 West																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																		
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PRIOR CROSSING PARKING STUDY NOVEMBER 2022 <table border="1"> <tr> <td>Total Spaces:</td> <td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td><td>12</td> </tr> <tr> <td>Staff Vehicles:</td> <td>3</td><td>2</td><td>2</td><td>3</td><td>3</td><td>2</td><td>3</td><td>2</td><td>3</td><td>3</td><td>2</td><td>3</td><td>2</td><td>3</td><td>2</td><td>3</td><td>3</td><td>2</td><td>3</td> </tr> <tr> <td>Resident Vehicles:</td> <td>8</td><td>4</td><td>5</td><td>5</td><td>5</td><td>4</td><td>8</td><td>5</td><td>5</td><td>5</td><td>4</td><td>5</td><td>5</td><td>5</td><td>4</td><td>5</td><td>8</td><td>4</td><td>4</td> </tr> <tr> <td>Nov-22</td> <td>7</td><td>8</td><td>9</td><td>10</td><td>11</td><td>14</td><td>15</td><td>16</td><td>17</td><td>18</td><td>21</td><td>22</td><td>23</td><td>24</td><td>25</td><td>28</td><td>29</td><td>30</td><td></td> </tr> <tr> <td>Morning Spaces in use</td> <td>3</td><td>6</td><td>7</td><td>8</td><td>6</td><td>3</td><td>6</td><td>6</td><td>7</td><td></td><td>4</td><td>7</td><td>n/a</td><td>n/a</td><td>n/a</td><td>4</td><td>2</td><td>3</td><td></td> </tr> <tr> <td>Morning Spaces open</td> <td>9</td><td>6</td><td>5</td><td>4</td><td>6</td><td>9</td><td>6</td><td>6</td><td>5</td><td></td><td>8</td><td>5</td><td>n/a</td><td>n/a</td><td>n/a</td><td>8</td><td>10</td><td>9</td><td></td> </tr> <tr> <td>Noon Spaces in use</td> <td>3</td><td>6</td><td>6</td><td>5</td><td>3</td><td>3</td><td>6</td><td>5</td><td>6</td><td></td><td>6</td><td>6</td><td>n/a</td><td>n/a</td><td>n/a</td><td>5</td><td>4</td><td>5</td><td></td> </tr> <tr> <td>Noon Spaces open</td> <td>9</td><td>6</td><td>6</td><td>7</td><td>9</td><td>9</td><td>6</td><td>7</td><td>6</td><td></td><td>6</td><td>6</td><td>n/a</td><td>n/a</td><td>n/a</td><td>7</td><td>8</td><td>7</td><td></td> </tr> <tr> <td>End of Workday Spaces in use</td> 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Eliminating the underground parking garage will create a potential savings of \$800,000 in construction costs.

Prepared by:
 Kirsten Spreck
 Director of Housing Development
 Beacon Interfaith Housing Collaborative
kspreck@beaconinterfaith.org



GENERAL NOTES

- 1.) Survey coordinate and bearing basis: Hennepin County ground feet, based on the Minnesota Coordinate System, Southern Zone, NAD83, 1986 adjustment.
- 2.) "No Buildings Observed" on the surveyed property in the process of conducting the fieldwork.
- 3.) Adjoining ownership information shown hereon was obtained from the Hennepin County Property Tax Information web site.
- 4.) The boundary shown is per the Certificates of Title listed. We have not been able to obtain any information from the Hennepin County Highway Department regarding Portland Avenue right of way. The boundary may be updated if we receive an updated Title Commitment that has more information regarding Portland Avenue.

UTILITY NOTES

- 1.) Utility information from plans and markings was combined with observed evidence of utilities to develop a view of the underground utilities shown hereon. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. In addition, Gopher State One Call locate requests from surveyors may be ignored or result in an incomplete response. Where additional or more detailed information is required, excavation and/or a private utility locate request may be necessary.
- 2.) Other underground utilities of which we are unaware may exist. Verify all utilities critical to construction or design.
- 3.) Some underground utility locations are shown as marked onsite by those utility companies whose locators responded to our Gopher State One Call, ticket numbers 230971408 and 230971431.
- 4.) Contact GOPHER STATE ONE CALL at 651-454-0002 (800-252-1166) for precise onsite location of utilities prior to any excavation.

FLOOD ZONE NOTE

- 1.) The subject property appears to lie within Zone X (Area of Minimal Flood Hazard) per the National Flood Insurance Program, Flood Insurance Rate Map No. 27053C0369F, dated November 4, 2016, Community No. 270180. This information was obtained from the FEMA Map Service Center web site.

PARKING

No designated parking spaces were observed on site.

AREAS

Area (Lots 31, 32, 33 and 34) = 28,478 square feet or 0.654 acres
 Area (Lot 59) = 1,650 square feet or 0.038 acres
 Gross Area (Lots 31, 32, 33, 34 & 59) = 30,128 square feet or 0.692 acres

BENCHMARKS (BM)
(NAVD 88)

- 1.) Top of top nut of fire hydrant in the southeast quadrant of 66th St. E. and Portland Ave. S.
Elevation = 851.97 feet
- 2.) Top of scribed "X" on the top of east side of concrete light pole base; 1st light pole south of bus shelter on the east side of Portland Ave. S.
Elevation = 849.41 feet
- 3.) Top of railroad spike in the west face of power pole with transformer on the east side of the site, middle power pole.
Elevation = 852.33 feet

DESCRIPTION OF PROPERTY SURVEYED

(Per Certificates of Title Nos. 1418469, 1418470, 1418471 and 1418472)

Lots 31, 32, 33, 34 and 59 of Auditor's Subdivision Number 340, Hennepin County, Minnesota.

PLAT RECORDING INFORMATION

AUDITOR'S SUBDIVISION NUMBER 340 was filed of record on September 12, 1946 in Book 116 of Plats, Page 33.

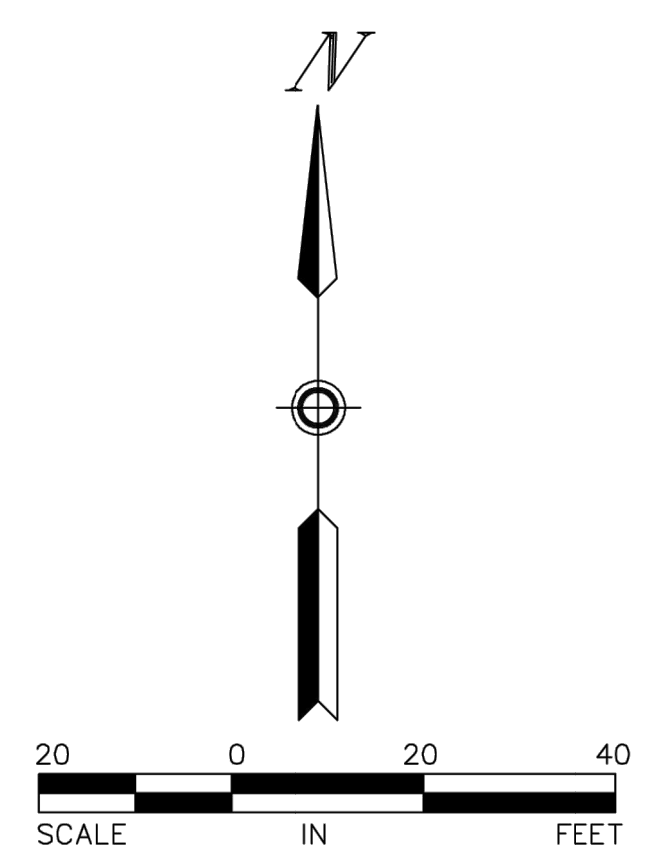
TITLE COMMITMENT

This survey was prepared without the benefit of current title work. Easements, appurtenances, and encumbrances may exist in addition to those shown hereon.

This survey is subject to revision upon receipt of a current title insurance commitment or attorney's title opinion.

LEGEND

- Denotes iron monument set marked with P.L.S. No. 44890
- Denotes found iron monument
- ⊙ Denotes 1.17 inch diameter copper magnetized marker with disc cap affixed stamped LS-44890 found
- BTL Denotes beavertail curb
- CB Denotes catch basin
- CIP Denotes cast iron pipe
- EB Denotes electric box
- EMH Denotes electric manhole
- G Denotes gutter elevation
- GW Denotes guy wire
- GYP Denotes disabled ramp
- HCR Denotes communication handhole
- HYD Denotes fire hydrant
- INV Denotes structure invert
- LP Denotes light pole
- OHU Denotes overhead utility lines
- (P) Denotes per plan
- PPT Denotes power pole
- PPT Denotes power pole with transformer
- RCP Denotes reinforced concrete pipe
- SAN Denotes sanitary manhole
- SAN S Denotes sanitary sewer
- SB Denotes soil boring
- ST S Denotes storm sewer
- TAD Denotes truncated domes plate
- TC Denotes top of concrete curb
- TCS Denotes traffic control sign
- UGE Denotes underground electric line
- W Denotes water line
- WV Denotes water valve
- EVG Denotes evergreen tree
- TR Denotes deciduous tree



I hereby certify that this survey, plan, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.
 Dated this 3rd day of May, 2023
 SUNDE LAND SURVEYING, LLC.
 By: *Leonard F. Carlson*
 Leonard F. Carlson, P.L.S. Minn. Lic. No. 44890

Revision	By	Date
	NDN	

Drawing Title:
BOUNDARY, LOCATION, TOPOGRAPHIC and UTILITY SURVEY FOR:
Beacon Interfaith Housing Collaborative
 OF: 6613-6625 Portland Ave S., Richfield, MN

SUNDE LAND SURVEYING
 Main Office: 9001 East Bloomington Freeway (35W) • Suite 118
 Bloomington, Minnesota 55420-3435
 952-881-2455 (Fax: 952-888-9526)
 www.sunde.com

Project: 2015-003-A	Bk/Pg: 1094/39	Date: 05/03/2023
Township: 028	Range: 24	Section: 26
File: 2015003A001.dwg		Sheet: 1 of 1

Design Development

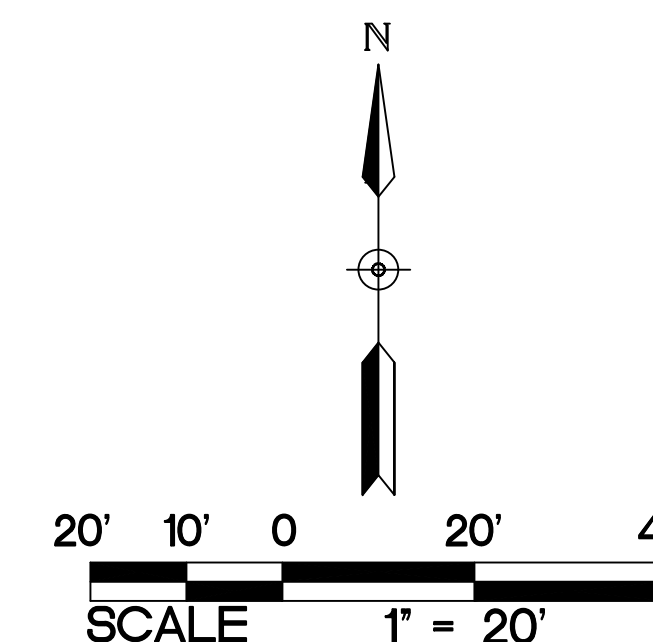
ISSUE / REVISION

Mark Date	Description
06/30/2023	Review Set
08/10/2023	City Review Comments

NOT FOR CONSTRUCTION

GRADING, DRAINAGE, AND EROSION CONTROL PLAN

C200



PROPOSED PLAN SYMBOLS

- CONSTRUCTION LIMITS: - - - - -
- SILTATION FENCE: X X X X X
- SEDIMENT CONTROL LOG: |||
- PROPERTY LINE: - - - - -
- SAWCUT LINE (APPROX.): - - - - -
- RETAINING WALL: [Symbol]
- PROPOSED CONTOUR: [Symbol]
- STORM SEWER: [Symbol]
- CATCH BASIN: [Symbol]
- MANHOLE: [Symbol]
- ROCK CONSTRUCTION ENTRANCE: [Symbol]
- DRAINAGE FLOW ARROW: [Symbol]
- SPOT ELEVATION: [Symbol]
- SOIL BORING: [Symbol]
- CONCRETE WASHOUT AREA: [Symbol]

*NOTE: CONSTRUCTION LIMITS ARE ANTICIPATED TO BE PROPERTY LINE UNLESS OTHERWISE SHOWN.

NOTE: STORM SEWER INLETS NOT SHOWN ON PLAN MAY RECEIVE RUNOFF FROM CONSTRUCTION ACTIVITIES. INSTALL INLET SEDIMENT PROTECTION PER DETAILS 4/C500 AND 5/C500 ON ALL STORM INLETS THAT MAY RECEIVE RUNOFF.

ABBREVIATIONS

BLDG	Building
BM	Benchmark
CB	Catch Basin
CONC	Concrete
ELEV	Elevation
EX	Existing
FFE	Finished Floor Elevation
INV	Invert
MAX	Maximum
MH	Manhole
MIN	Minimum
PVC	Polyvinyl Chloride
RCP	Reinforced Concrete Pipe
SAN	Sanitary Sewer
ST	Storm Sewer
STRM	Storm Sewer Structure
UGE	Underground Electrical Line
W.O.	Washout

WALKWAY NOTES:

1. GRADING CONTRACTOR IS TO COORDINATE WITH PAVING CONTRACTOR SO THAT ALL LANDINGS ARE SLOPED PER CODE.
2. ALL SIDEWALK LONGITUDINAL AND TRANSVERSE SLOPES ARE TO BE PER CODE.

APPROXIMATE DISTURBED AREA IS 0.65 ACRES

NOTE: WHERE GRADE OVER EXISTING SMALL UTILITIES IS PROPOSED TO BE LOWERED, CONTRACTOR SHALL COORDINATE WITH UTILITY OWNER FOR THE LOWERING OF THE EXISTING UTILITY TO PROVIDE THE MINIMUM COVER REQUIRED AT NO ADDITIONAL COST TO THE OWNER.

NOTE: PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL POTHOLE AND DETERMINE THE LOCATION AND ELEVATION OF EXISTING UNDERGROUND UTILITIES. CONTACT CIVIL ENGINEER IF EXISTING UTILITIES TO REMAIN WILL BE IMPACTED BY PROPOSED WORK.

- KEYED NOTES**
- KEYED NOTES ARE DENOTED BY [Symbol] ON PLAN.
- 1] INSTALL ROCK CONSTRUCTION ENTRANCE. REFER TO DETAIL 1/C500.
 - 2] INSTALL PERIMETER EROSION CONTROL. REFER TO DETAILS 2/C500 AND 3/C500.
 - 3] INSTALL INLET SEDIMENT PROTECTION. REFER TO DETAILS 4/C500 AND 5/C500.
 - 4] APPROXIMATE LOCATION OF TEMPORARY CONTAINED CONCRETE WASH OUT BIN. REFER TO THE MINNESOTA'S NPDES/SDS GENERAL STORMWATER PERMIT FOR CONSTRUCTION ACTIVITY FOR MORE DETAILS. SELF CONTAINED CONCRETE WASHOUTS ON CONCRETE DELIVERY TRUCKS IS AN ACCEPTABLE ALTERNATIVE TO ON-SITE CONTAINMENT.
 - 5] THE LINED FILTRATION BASIN IS TO BE CONSTRUCTED AT THE END OF GRADING OPERATIONS ONCE THE TRIBUTARY AREA'S FINAL STABILIZATION HAS BEEN INSTALLED. REFER TO DETAIL 1/C504 FOR RAIN GARDEN'S CROSS SECTION. CONSTRUCTION TRAFFIC IN THE BASIN AREA IS NOT ALLOWED AFTER AREA HAS BEEN EXCAVATED. PRIOR TO FINAL STABILIZATION, LOOSEN SOIL WITH MECHANICAL TILLER. THE BASIN AREA(S) ARE NOT APPROVED BORROW SITES AND ARE NOT TO BE USED FOR TEMPORARY SEDIMENT BASIN(S) ONCE BASIN(S) SUBGRADE ELEVATION HAS BEEN EXCAVATED. CONTRACTOR SHALL ENSURE THAT BASIN(S) FILTRATE AT A MINIMUM RATE OF 1-INCH PER HOUR USING A DOUBLE RING INFILTROMETER TEST BEFORE FINAL ACCEPTANCE. DOUBLE RING INFILTROMETER TEST SHALL BE SIGNED BY A REGISTERED GEOTECHNICAL ENGINEER AND SUBMITTED TO THE CITY AND ENGINEER FOR REVIEW BEFORE FINAL APPROVAL.

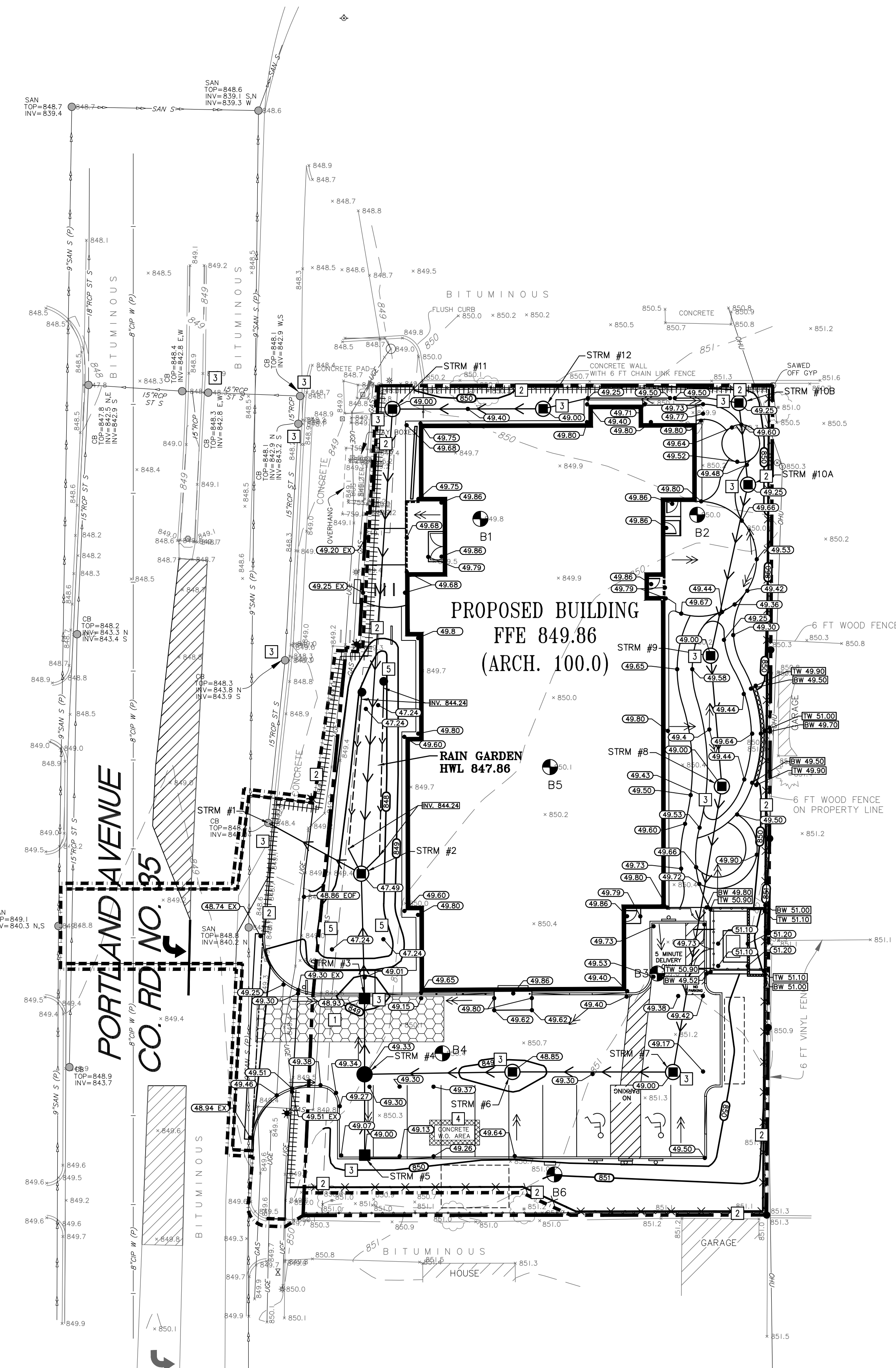
- EROSION CONTROL NOTES:**
1. ALL EROSION CONTROL FACILITIES SHALL BE INSTALLED PRIOR TO ANY SITE GRADING OPERATIONS. THE CITY ENGINEERING DEPARTMENT AND MINNEHAHA CREEK WATERSHED DISTRICT MUST BE NOTIFIED UPON COMPLETION OF THE INSTALLATION OF THE REQUIRED EROSION CONTROL FACILITIES AND PRIOR TO ANY GRADING OPERATION BEING COMMENCED. THE CONTRACTOR IS RESPONSIBLE FOR SCHEDULING A PRE-CONSTRUCTION GRADING MEETING ON-SITE WITH THE CITY AND MINNEHAHA CREEK WATERSHED DISTRICT. IF DAMAGED OR REMOVED DURING CONSTRUCTION, ALL EROSION CONTROL FACILITIES SHALL BE RESTORED AND IN PLACE AT THE END OF EACH DAY.
 2. ANY EROSION CONTROL FACILITIES DEEMED NECESSARY BY THE CITY OR MINNEHAHA CREEK WATERSHED DISTRICT, BEFORE, DURING OR AFTER THE GRADING ACTIVITIES, SHALL BE INSTALLED AT THEIR REQUEST. WEEKLY SITE INSPECTIONS OF THE EROSION CONTROL FACILITIES IS REQUIRED.
 3. NO DEVIATIONS SHALL BE MADE FROM THE ELEVATIONS SHOWN ON THE APPROVED GRADING PLAN WITHOUT PRIOR APPROVAL FROM THE CIVIL ENGINEER.
 4. FLOWS FROM DIVERSION CHANNELS OR PIPES (TEMPORARY OR PERMANENT) SHALL BE ROUTED TO SEDIMENTATION BASINS OR APPROPRIATE ENERGY DISSIPATORS TO PREVENT TRANSPORT OF SEDIMENT TO OUTFLOW TO LATERAL CONVEYORS AND TO PREVENT EROSION AND SEDIMENTATION WHEN RUNOFF FLOWS INTO THESE CONVEYORS.
 5. SITE ACCESS ROADS SHALL BE GRADED OR OTHERWISE PROTECTED WITH SILT FENCES, DIVERSION CHANNELS, OR DIKES AND PIPES TO PREVENT SEDIMENT FROM EXITING THE SITE VIA THE ACCESS ROADS. SITE-ACCESS ROADS/DRIVEWAYS SHALL BE SURFACED WITH CRUSHED ROCK WHERE THEY ADJOIN EXISTING PAVED ROADWAYS.
 6. SOILS TRACKED FROM THE SITE BY MOTOR VEHICLES OR EQUIPMENT SHALL BE CLEANED DAILY FROM PAVED ROADWAY SURFACES, OR MORE FREQUENTLY IF REQUESTED BY CITY OR MINNEHAHA CREEK WATERSHED DISTRICT, THROUGHOUT THE DURATION OF CONSTRUCTION.
 7. DUST CONTROL MEASURES SHALL BE PERFORMED PERIODICALLY WHEN CONDITIONS REQUIRE AND/OR AS DIRECTED BY THE CITY OR MINNEHAHA CREEK WATERSHED DISTRICT.
 8. ALL EROSION CONTROL MEASURES SHALL BE USED AND MAINTAINED FOR THE DURATION OF SITE CONSTRUCTION. IF CONSTRUCTION OPERATIONS OR NATURAL EVENTS DAMAGE OR INTERFERE WITH THESE EROSION CONTROL MEASURES, THEY SHALL BE RESTORED TO SERVE THEIR INTENDED FUNCTION AT THE END OF EACH DAY OR AS SOON AS FIELD CONDITIONS ALLOW ACCESS.
 9. ALL AREAS DISTURBED DURING CONSTRUCTION SHALL BE RESTORED AS SOON AS POSSIBLE. ANY AREAS WHICH HAVE BEEN FINISHED GRADED OR AREAS THAT HAVE BEEN DISTURBED AND FOR WHICH GRADING OR SITE BUILDING CONSTRUCTION OPERATIONS ARE NOT ACTIVELY UNDERWAY SHALL BE SEEDED AND MULCHED AS SET FORTH IN THE FOLLOWING PARAGRAPHS WITHIN 14 DAYS:
 - A. ALL SEEDED AREAS SHALL BE EITHER MULCHED AND DISC-ANCHORED OR COVERED BY FIBROUS BLANKETS TO PROTECT SEEDS AND LIMIT EROSION. TEMPORARY STRAW MULCH SHALL BE DISC-ANCHORED AND APPLIED AT A UNIFORM RATE OF NOT LESS THAN TWO TONS PER ACRE AND NOT LESS THAN 80% COVERAGE.
 - B. IF THE GRADED AREA IS ANTICIPATED TO BE RE-DISTURBED/DEVELOPED WITHIN SIX MONTHS, PROVIDE A TEMPORARY VEGETATIVE COVER CONSISTING OF MINNESOTA DEPARTMENT OF TRANSPORTATION (MNDOT) SEED MIXTURE 21-111 (OATS), OR 21-112 (WINTER WHEAT), AT A RATE OF 100 POUNDS PER ACRE.
 - C. IF GRADED AREA WILL NOT BE DEVELOPED FOR A PERIOD GREATER THAN SIX MONTHS, PROVIDE A SEMI-PERMANENT VEGETATIVE COVER OF SEED MIXTURE MNDOT 22-112 AT A RATE OF 40 POUNDS PER ACRE.
 - D. GRADING BONDS OR THE EQUIVALENT SECURITIES SHALL BE RETAINED UNTIL TURF HAS GERMINATED AND SURVIVED A 60-DAY GROWING PERIOD.
 - E. REFER TO THE LANDSCAPE PLAN FOR PERMANENT TURF RESTORATION.
 - F. MULCH, HYDROMULCH, AND TACKIFIERS MAY NOT BE USED FOR STABILIZATION IN SWALES OR DRAINAGE DITCHES UNLESS THE LONGITUDINAL SLOPE IS LESS THAN 2 PERCENT.
 - G. RUNOFF SHALL BE PREVENTED FROM ENTERING ALL STORM SEWER CATCH BASINS PROVIDING THEY ARE NOT NEEDED DURING CONSTRUCTION. WHERE STORM SEWER CATCH BASINS ARE NECESSARY FOR SITE DRAINAGE DURING CONSTRUCTION, SEDIMENT PROTECTION DEVICES AS DETAILED SHALL BE INSTALLED AND MAINTAINED AROUND ALL CATCH BASINS UNTIL THE TRIBUTARY AREA TO THE CATCH BASIN IS RESTORED.
 11. GRADING ACTIVITIES PROPOSED TO BEGIN AFTER OCTOBER 15 WILL REQUIRE AN APPROVED PHASING SCHEDULE. THE AREA OF LAND THAT THE CITY WILL ALLOW TO BE DISTURBED AT THIS TIME OF YEAR WILL BE SEVERELY LIMITED. THE CITY WILL ALSO REQUIRE ADDITIONAL EROSION CONTROL DEVICES, I.E., TEMPORARY SEDIMENT BASINS, DORMANT SEEDING AND HIGH RATES OF APPLICATION OF BOTH SEED AND MULCH.
 12. TO MINIMIZE EROSION, ALL 3:1 SLOPES SHALL BE COVERED WITH A MN/DOT 3885 CATEGORY 20 STRAW EROSION CONTROL BLANKETS OR STAKED SOD.
 13. ACCUMULATION OF ALL SEDIMENT OCCURRING IN STORM SEWERS SHALL BE REMOVED PRIOR TO, DURING AND AFTER COMPLETION OF GRADING ACTIVITIES.
 14. EROSION CONTROL ITEMS AND DEVICES SHALL BE REMOVED ONLY AFTER THE AREA HAS RECEIVED FINAL STABILIZATION OR AS DIRECTED BY THE CITY AND/OR MINNEHAHA CREEK WATERSHED DISTRICT.

- RICHFIELD GENERAL NOTES:**
1. PERMITS ARE REQUIRED FROM RICHFIELD ENGINEERING AND HENNEPIN COUNTY FOR ANY EXCAVATION IN THE ROW, OBSTRUCTION OF PUBLIC ROADWAYS, PARKING LANES, PUBLIC SIDEWALK, OR PERMANENT STRUCTURES CONSTRUCTED IN RIGHT OF WAY. PERMITS FORMS AND INFORMATION AVAILABLE AT WWW.RICHFIELDMN.GOV/PWPERMITS.
 2. THE CITY OF RICHFIELD REQUIRES A FULL AS-BUILT PLAN SET/RECORD PLANS TO RELEASE ESCROW IN THE AMOUNT OF \$15,000. RECORD DRAWINGS ARE REQUIRED PRIOR TO FINAL C.O. ISSUE. SUBMIT PDF OF COMPLETE CIVIL AND SITE PLAN SET INCLUDING ALL UPDATES WITH AS-BUILT INFORMATION OF ALL UTILITIES INSTALLED AND REMOVED AND LIGHTING INSTALLED.

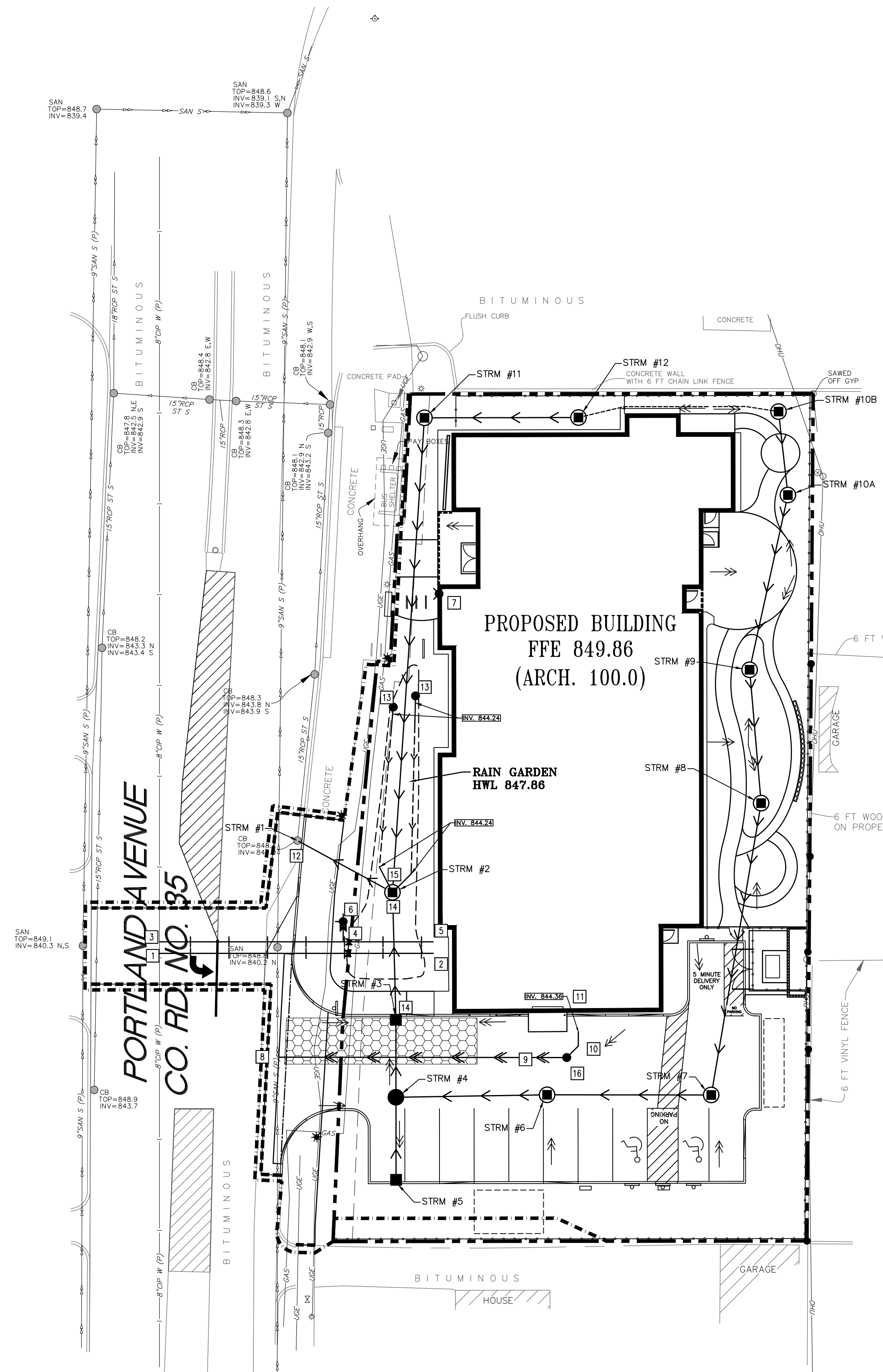
RETAINING WALL NOTES:

1. ALL TOP AND BOTTOM ELEVATIONS CORRESPOND TO THE RESPECTIVE GRADE ELEVATIONS ON EACH SIDE OF THE WALL.
2. THE BOTTOM ELEVATION IS THE ELEVATION OF THE LOW-GRADE SIDE OF THE WALL, NOT THE TOP ELEVATION OF THE BURIED BLOCK COURSE.

1 GRADING, DRAINAGE, AND EROSION CONTROL PLAN
1" = 20'



Drawn: 2022/06/01, Mauer, Shuman & Robinson, Ltd. 5/1/2023 10:00 AM 1/23299.50/Grading, Drainage, and Erosion Control Plan - C200.dwg



1
C300 UTILITY PLAN
1" = 20'

UTILITY NOTES FOR WORK IN PUBLIC RIGHT-OF-WAY:

1. FOLLOW ALL CITY OF RICHFIELD AND HENNEPIN COUNTY STANDARDS AND SPECIFICATIONS.
2. PRIOR TO CONSTRUCTION, CONTRACTORS ARE TO COORDINATE ALL WORK WITHIN RIGHT-OF-WAY AND OBTAIN ALL APPLICABLE PERMITS.

PROPOSED PLAN SYMBOLS

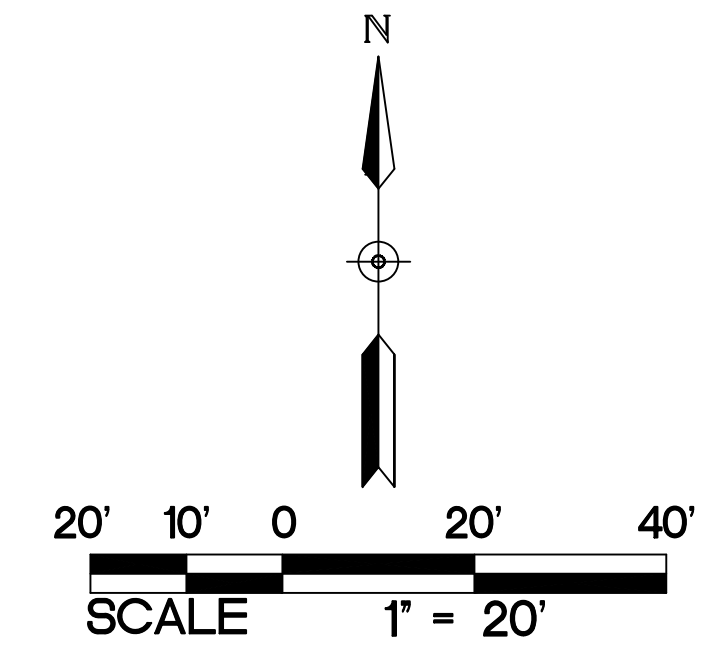
CONSTRUCTION LIMITS	---
PROPERTY LINE	— — — — —
SAWCUT LINE (APPROX.)	- - - - -
RETAINING WALL	⌢ — ⌢ — ⌢ — ⌢ — ⌢ —
SANITARY SEWER	—●—
STORM SEWER	—○—
WATER PIPE	—+—
FIRE HYDRANT	⊙
FIRE CONNECTION	⊙
GATE VALVE	⊙
CATCH BASIN	⊙
MANHOLE	⊙
CLEANOUT	⊙
SEWER INVERT ELEVATION	INV. 844.38

ABBREVIATIONS

BLDG	Building
BM	Benchmark
CB	Catch Basin
CONC	Concrete
DIP	Ductile Iron Pipe
ELEV	Elevation
EX	Existing
FFE	Finished Floor Elevation
HDPE	High Density Polyethylene
INV	Invert
MAX	Maximum
MIN	Minimum
OHL	Overhead Utility Line
PIV	Post Indicator Valve
PVC	Polyvinyl Chloride
RCP	Reinforced Concrete Pipe
UGE	Underground Electric

*NOTE: CONSTRUCTION LIMITS ARE ANTICIPATED TO BE PROPERTY LINE UNLESS OTHERWISE SHOWN.

- UTILITY NOTES:**
1. CONTRACTOR SHALL FOLLOW ALL CITY OF RICHFIELD STANDARDS AND SPECIFICATIONS.
 2. COORDINATE SERVICE CONNECTION LOCATIONS AT THE BUILDING WITH THE MECHANICAL CONTRACTOR PRIOR TO CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR UNCOORDINATED WORK.
 3. COORDINATE UTILITY INSTALLATION WITH STRUCTURAL PRIOR TO START OF CONSTRUCTION. UTILITIES SHALL NOT BE INSTALLED WITHIN THE ZONE OF INFLUENCE OF ANY STRUCTURAL ELEMENTS. NO ADDITIONAL COMPENSATION WILL BE PROVIDED FOR UNCOORDINATED WORK.
 4. ALL SEWER SERVICE CONNECTIONS WITH LESS THAN 5 FEET OF COVER OVER THE TOP OF PIPE SHALL BE INSULATED. INSULATION SHALL BE INSTALLED FROM THE CONNECTION OF THE SERVICE AT THE BUILDING TO THE POINT WHICH THE SERVICE ATTAINS 5 FEET OF COVER. CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION FROM ARCHITECT OR ENGINEER PRIOR TO INSTALLATION OF INSULATION.
 5. PROTECT ALL EXISTING STRUCTURES AND UTILITIES WHICH ARE NOT SCHEDULED TO BE REMOVED.
 6. ALL SEWER AND WATER CROSSINGS SHALL HAVE A MINIMUM VERTICAL SEPARATION OF 1.5 FEET AND HORIZONTAL SEPARATION OF 10 FEET. FOLLOW ALL HEALTH DEPARTMENT AND CITY OF RICHFIELD AND HENNEPIN COUNTY STANDARDS.
 7. ALL WATER SERVICES SHALL BE DUCTILE IRON PIPE, CLASS 52, UNLESS NOTED OTHERWISE. THE DOMESTIC WATER SERVICE SHALL BE TYPE K COPPER.
 8. ALL WATER MAIN SHALL HAVE A MINIMUM DEPTH OF COVER OF 7.5 FEET OVER TOP OF WATER MAIN.
 9. PROVIDE THRUST BLOCKING ON ALL WATER MAIN PER CITY OF RICHFIELD. PROVIDE MECHANICAL JOINT RESTRAINTS ON ALL BENDS, VALVES, TEES, PLUGS, AND HYDRANT LEADS.
 10. SANITARY SEWER PIPING SHALL BE SDR 35 PVC UNLESS NOTED OTHERWISE.
 11. STORM SEWER PIPING SHALL BE ASTM D2665 OR D1785 SCHEDULE 40 PVC.
 12. CONTRACTORS SHALL COORDINATE ALL WORK WITH GAS, ELECTRIC, TELEVISION AND TELEPHONE COMPANIES PRIOR TO START OF CONSTRUCTION.
 13. WHERE PROPOSED GRADE OVER EXISTING SMALL UTILITIES IS PROPOSED TO BE LOWERED, CONTRACTOR SHALL COORDINATE WITH UTILITY OWNER FOR THE LOWERING OF THE EXISTING UTILITY TO PROVIDE THE MINIMUM COVER REQUIRED AT NO ADDITIONAL COST TO THE OWNER.
 14. ALL PORTIONS OF THE STORM SEWER SYSTEM LOCATED WITHIN 10- FEET OF THE BUILDING OR WATER SERVICE LINE SHALL BE TESTED IN ACCORDANCE WITH MN PLUMBING CODE. PIPING MATERIAL SHALL BE SCHEDULE 40 PVC.
- ALL JOINTS AND CONNECTIONS IN THE STORM SEWER SYSTEM SHALL BE GAS TIGHT OR WATER TIGHT IN ACCORDANCE TO MN PLUMBING CODE. APPROVED RESILIENT RUBBER JOINTS MUST BE USED TO MAKE WATER TIGHT CONNECTIONS TO MANHOLES, CATCH BASINS, AND OTHER STRUCTURES. RESILIENT WATER-STOP GROUTING RINGS ARE AN ACCEPTABLE ALTERNATIVE. CEMENT MORTAR JOINTS ARE PERMITTED ONLY FOR REPAIRS AND CONNECTIONS OF EXISTING LINES CONSTRUCTED WITH SUCH JOINTS.**
15. SERVICES SHALL BE TESTED PER CITY OF RICHFIELD SPECIFICATIONS.



- KEYED NOTES**
- KEYED NOTES ARE DENOTED BY [] ON PLAN.
1. INSTALL 2-INCH CORPORATION STOP. CURB BOX TO BE INSTALLED AT PROPOSED PROPERTY LINE. REFER TO DETAIL 4/C501.
 2. STUB 2-INCH DOMESTIC WATERLINE TO WITHIN 5- FEET OF THE PROPOSED BUILDING. TOP OF WATER SERVICE SHALL BE 7.5 FEET BELOW FINISHED GRADE AT THE PROPOSED CONNECTION POINT. COORDINATE EXACT LOCATION WITH MECHANICAL AND STRUCTURAL PLANS PRIOR TO THE START OF CONSTRUCTION.
 3. INSTALL 6-INCH WET TAP. GATE VALVE TO BE INSTALL AT PROPOSED PROPERTY LINE. REFER TO DETAIL 4/C501.
 4. INSTALL 6-INCH BY 6-INCH TEE.
 5. STUB 6-INCH FIRE PROTECTION LINE TO WITHIN 5- FEET OF THE PROPOSED BUILDING. TOP OF WATER SERVICE SHALL BE 7.5 FEET BELOW FINISHED GRADE AT THE PROPOSED CONNECTION POINT. COORDINATE EXACT LOCATION WITH MECHANICAL AND STRUCTURAL PLANS PRIOR TO THE START OF CONSTRUCTION.
 6. INSTALL FIRE HYDRANT AND GATE VALVE. REFER TO DETAIL 6/C501.
 7. INSTALL FIRE DEPARTMENT CONNECTION. REFER TO MECHANICAL PLANS.
 8. CONNECT TO EXISTING 9-INCH CLAY PIPE WITH 9-INCH BY 6-INCH WYE. APPROXIMATE INVERT OF EXISTING PIPE IS 840.37. CONTRACTOR SHALL NOTIFY ENGINEER OF EXACT INVERT PRIOR TO INSTALLATION OF SANITARY SEWER. REFER TO DETAIL 1/C501.
 9. INSTALL 83- FEET OF 6-INCH PVC PIPE AT MINIMUM 2.00% SLOPE TO INVERT OF 844.36. STUB TO WITHIN 5- FEET OF PROPOSED BUILDING. COORDINATE EXACT LOCATION AND INVERT ELEVATION WITH MECHANICAL CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.
 10. INSTALL 6-INCH 45- DEGREE BEND.
 11. LOCATION OF PROPOSED 6-INCH SANITARY SEWER SERVICE. COORDINATE EXACT LOCATION AND INVERT ELEVATION WITH MECHANICAL CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.
 12. CORE DRILL EXISTING MANHOLE FOR PROPOSED PIPE CONNECTION. GROUT SEAL AROUND OPENING. REFER TO STORM SEWER TABLE FOR INVERT.
 13. INSTALL DRAIN TILE CLEAN OUT. REFER TO DETAIL 3/C504.
 14. INSTALL SUMP CATCH BASIN WITH SNUOT, ENVIROHOOD, OR APPROVED EQUAL. REFER TO DETAIL 4/C504.
 15. INSTALL BACKFLOW PREVENTION VALVE FOR DRAIN TILE CONNECTIONS.
 16. INSTALL SANITARY SEWER CLEAN OUT. REFER TO DETAIL 5/C504.

- RICHFIELD GENERAL NOTES:**
1. PERMITS ARE REQUIRED FROM RICHFIELD ENGINEERING AND HENNEPIN COUNTY FOR ANY EXCAVATION IN THE ROW, OBSTRUCTION OF PUBLIC ROADWAYS, PARKING LANES, PUBLIC SIDEWALK, OR PERMANENT STRUCTURES CONSTRUCTED IN RIGHT OF WAY. PERMITS FORMS AND INFORMATION AVAILABLE AT WWW.RICHFIELDMN.GOV/PWPERMITS.
 2. THE CITY OF RICHFIELD REQUIRES A FULL AS-BUILT PLAN SET/RECORD PLANS TO RELEASE ESCROW IN THE AMOUNT OF \$15,000. RECORD DRAWINGS ARE REQUIRED PRIOR TO FINAL C.O. ISSUE. SUBMIT PDF OF COMPLETE CIVIL AND SITE PLAN SET INCLUDING ALL UPDATES WITH AS-BUILT INFORMATION OF ALL UTILITIES INSTALLED AND REMOVED AND LIGHTING INSTALLED.
 3. WATER AND SANITARY SEWER SERVICES INCLUDING HYDRANTS, CLEAN-OUTS, AND SHUTOFF VALVE SHALL CONFORM TO CITY SPECIFICATIONS.
 4. ENGINEERING MUST BE PRESENT TO INSPECT ALL CONCRETE FORMS WITHIN ROW PRIOR TO CONCRETE POUR. MINIMUM 24 HOUR NOTICE REQUIRED.
 5. DRIVEWAYS MUST CONFORM TO CITY DETAIL STR-02.

STORM SEWER TABLE						
STRUCTURE ID	STRUCTURE DIMENSION (INCHES)	NEENAH CASTING TYPE	RIM ELEVATION	INVERT ELEVATION(S)	PIPE LENGTH, DIAMETER, SLOPE & NEXT UPSTREAM STRUCTURE	
STRM #1	EX. CBMH	R-1733	845.77	SE = 844.10	28 L.F. OF 12" RCP @ 0.50% STRM #2	
STRM #2	48" SUMP CB MH	R-2390	847.49	NW = 844.24 S = 844.52 N = 844.52	32 L.F. OF 12" RCP @ 0.50% STRM #3 124 L.F. OF 8" PVC @ 0.91% STRM #11	
STRM #3	48" SUMP MH	R-3067-C	848.85	N = 844.68 S = 844.68	21 L.F. OF 12" RCP @ 0.50% STRM #4	
STRM #4	48" MH	R-1733	849.33	N = 844.78 S = 844.78 E = 844.78	22 L.F. OF 12" RCP @ 0.50% STRM #5 39 L.F. OF 12" RCP @ 0.50% STRM #6	
STRM #5	24"x36" CB	R-3067	849.00	N = 844.89	-----	
STRM #6	48" CB MH	R-2390	848.85	W = 844.98 E = 844.98	43 L.F. OF 12" RCP @ 0.50% STRM #7	
STRM #7	48" CB MH	R-2390	849.00	W = 845.19 N = 845.19	77 L.F. OF 8" PVC @ 0.57% STRM #8	
STRM #8	YARD DRAIN	ADS H-20	847.15	S = 845.63 N = 845.63	35 L.F. OF 8" PVC @ 0.72% STRM #9	
STRM #9	YARD DRAIN	ADS H-20	847.59	S = 845.88 N = 845.88	46 L.F. OF 8" PVC @ 0.52% STRM #10A	
STRM #10A	YARD DRAIN	ADS H-20	849.25	S = 846.12 N = 846.12	22 L.F. OF 8" PVC @ 0.60% STRM #10B	
STRM #10B	YARD DRAIN	ADS H-20	849.25	S = 846.25	-----	
STRM #11	YARD DRAIN	ADS H-20	849.00	S = 845.65 E = 845.65	40 L.F. OF 8" PVC @ 0.88% STRM #12	
STRM #12	YARD DRAIN	ADS H-20	849.00	W = 846.00	-----	

Aster Commons
6613 Portland Avenue South
Richfield, MN 55423

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the Laws of the State of Minnesota.

Signature _____
Print Name Joel W. Maier
Date 06/30/2023 License No 19181

Design Development

ISSUE / REVISION	
Mark Date	Description
06/30/2023	Review Set
08/10/2023	City Review Comments

NOT FOR CONSTRUCTION

UTILITY PLAN

C300

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Aster Commons
6613 Portland Avenue South
Richfield, MN 55423

Project No: 2022018

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Signature _____

Print Name Joel W. Maier

Date 06/30/2023 License No 19181

Design Development

ISSUE / REVISION

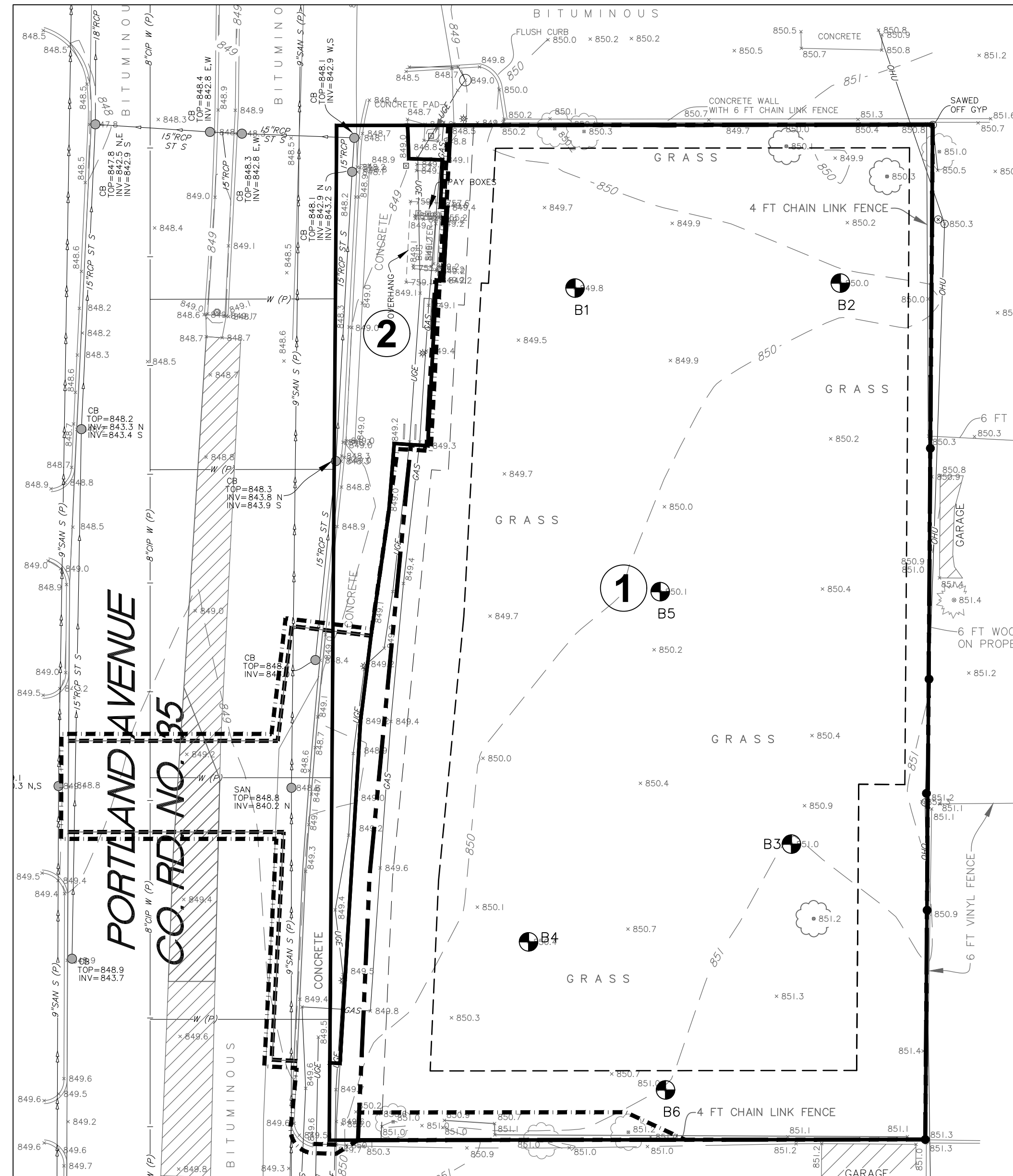
Mark Date	Description
06/30/2023	Review Set
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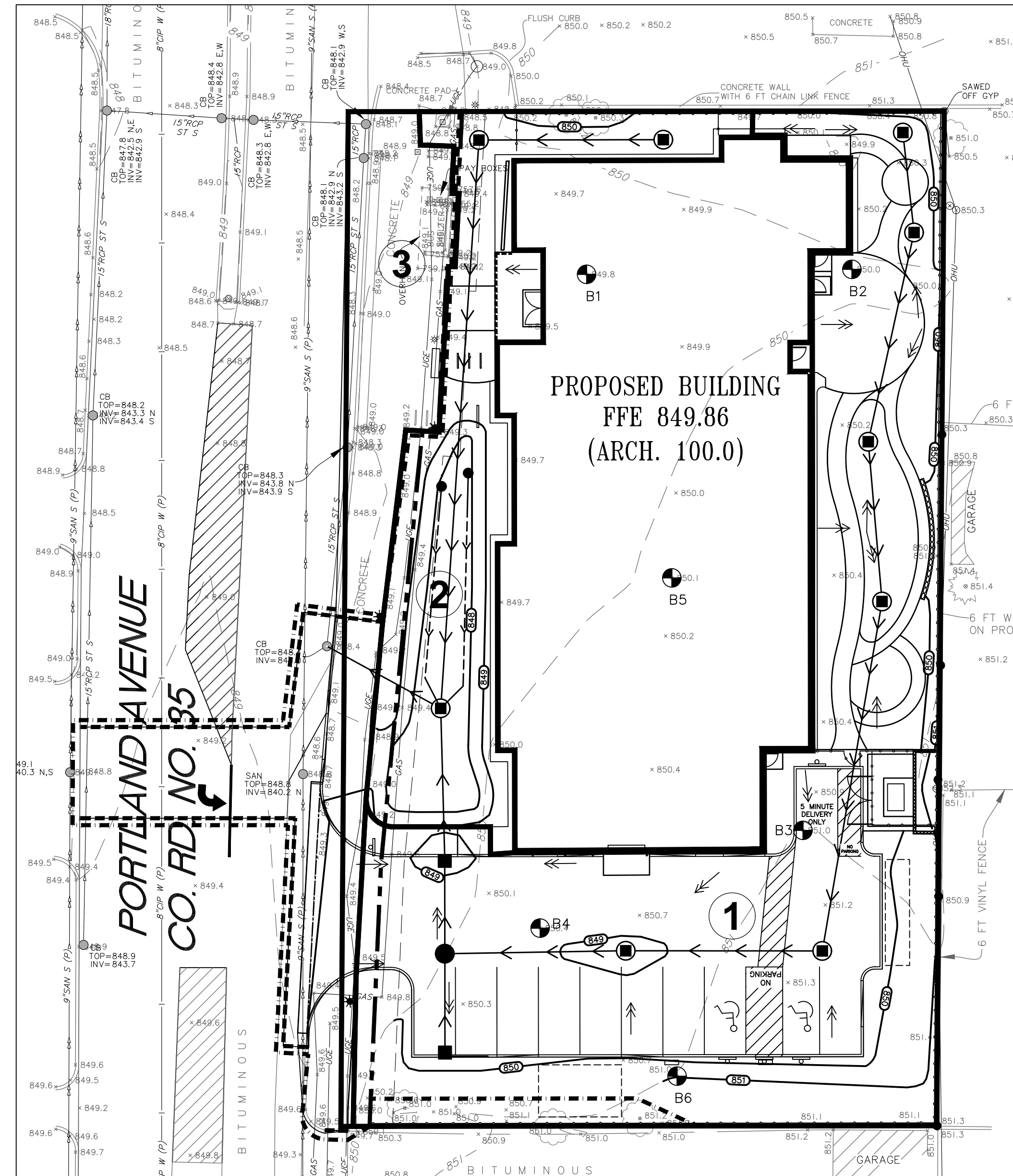
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STORMWATER SUMMARY

C600



1 EXISTING CONDITIONS
C6.0 1"=20'



2 PROPOSED CONDITIONS
C6.0 1"=20'

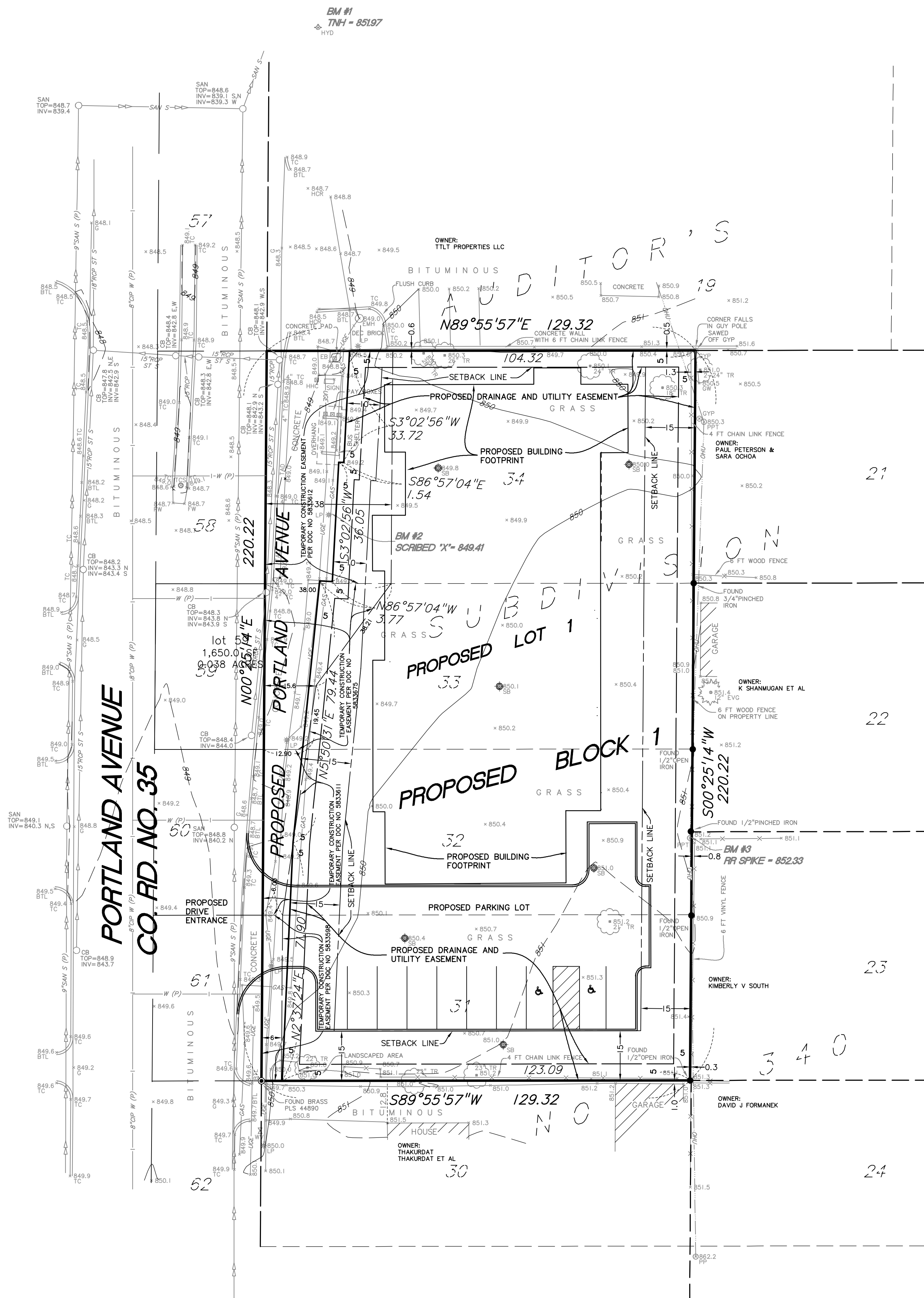


VICINITY MAP
RICHFIELD, MINNESOTA

DRAINAGE AREA	IMPERVIOUS AREA (ACRES)	PERVIOUS AREA (ACRES)	TOTAL AREA (ACRES)	Q OUT (CFS) STORM EVENT			ROUTING
				2-YEAR (2.84")	10-YEAR (4.25")	100-YEAR (7.50")	
				1	0.00	0.60	
2	0.05	0.00	0.05	0.24	0.36	0.64	PORTLAND AVE
TOTAL	0.05	0.60	0.65	0.39	1.07	3.24	

DRAINAGE AREA	IMPERVIOUS AREA (ACRES)	PERVIOUS AREA (ACRES)	TOTAL AREA (ACRES)	Q OUT (CFS) STORM EVENT			ROUTING
				2-YEAR (2.84")	10-YEAR (4.25")	100-YEAR (7.50")	
				1	0.16	0.12	
2	0.24	0.08	0.32	—	—	—	RAIN GARDEN
3	0.06	0.00	0.06	0.24	0.36	0.64	PORTLAND AVE
RAIN GARDEN	—	—	—	0.69	1.51	3.07	PORTLAND AVE
TOTAL	0.45	0.20	0.65	1.68	3.15	6.33	

	STORMWATER RUNOFF SUMMARY		
	2-YR STORM (2.84") RUNOFF (CFS)	10-YR STORM (4.25") RUNOFF (CFS)	100-YR STORM (7.50") RUNOFF (CFS)
EXISTING SITE	0.39	1.07	3.24
PROPOSED SITE	1.68	3.15	6.33



GENERAL NOTES

- 1.) Survey coordinate and bearing basis: Hennepin County ground feet, based on the Minnesota Coordinate System, Southern Zone, NAD83, 1986 adjustment.
- 2.) "No Buildings Observed" on the surveyed property in the process of conducting the fieldwork.
- 3.) Adjoining ownership information shown hereon was obtained from the Hennepin County Property Tax Information web site.
- 4.) The boundary shown is per the Certificates of Title listed. We have not been able to obtain any information from the Hennepin County Highway Department regarding Portland Avenue right of way. The boundary may be updated if we receive an updated Title Commitment that has more information regarding Portland Avenue.
- 5.) Physical features shown on the survey are per the last field visit on April 28, 2023.

UTILITY NOTES

- 1.) Utility information from plans and markings was combined with observed evidence of utilities to develop a view of the underground utilities shown hereon. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. In addition, Gopher State One Call locate requests from surveyors may be ignored or result in an incomplete response. Where additional or more detailed information is required, excavation and/or a private utility locate request may be necessary.
- 2.) Other underground utilities of which we are unaware may exist. Verify all utilities critical to construction or design.
- 3.) Some underground utility locations are shown as marked onsite by those utility companies whose locators responded to our Gopher State One Call, ticket numbers 230971408 and 230971431.
- 4.) Contact GOPHER STATE ONE CALL at 651-454-0002 (800-252-1166) for precise onsite location of utilities prior to any excavation.

FLOOD ZONE NOTE

- 1.) The subject property appears to lie within Zone X (Area of Minimal Flood Hazard) per the National Flood Insurance Program, Flood Insurance Rate Map No. 27053C0369F, dated November 4, 2016, Community No. 270180. This information was obtained from the FEMA Map Service Center web site.

PARKING

No designated parking spaces were observed on site.

AREAS

Existing Area = 28,478 square feet or 0.654 acres
 Proposed Lot 1, Block 1 = 25,355 square feet or 0.582 acres

BENCHMARKS (BM)

- 1.) Top of top nut of fire hydrant in the southeast quadrant of 66th St. E. and Portland Ave. S.
Elevation = 851.97 feet
- 2.) Top of scribed "X" on the top of east side of concrete light pole base; 1st light pole south of bus shelter on the east side of Portland Ave. S.
Elevation = 849.41 feet
- 3.) Top of railroad spike in the west face of power pole with transformer on the east side of the site, middle power pole.
Elevation = 852.33 feet

LEGEND

- Denotes found iron monument
- ⊙ Denotes 1.17 inch diameter copper magnetized marker with disc cap affixed stamped LS-44890 found
- BTL Denotes beavertail curb
- CB Denotes catch basin
- CIP Denotes cast iron pipe
- EB Denotes electric box
- EMH Denotes electric manhole
- G Denotes gutter elevation
- GW Denotes guy wire
- GYP Denotes guy pole
- HCR Denotes disabled ramp
- HHC Denotes communication handhole
- HYD Denotes fire hydrant
- INV Denotes structure invert
- LP Denotes light pole
- OHU Denotes overhead utility lines
- (P) Denotes per plan
- PP Denotes power pole
- PPT Denotes power pole with transformer
- RCP Denotes reinforced concrete pipe
- SAN Denotes sanitary manhole
- SAN S Denotes sanitary sewer
- SB Denotes soil boring
- ST S Denotes storm sewer
- TAD Denotes truncated domes plate
- TC Denotes top of concrete curb
- TCS Denotes traffic control sign
- UGE Denotes underground electric line
- W Denotes water line
- WV Denotes water valve
- EVG Denotes evergreen tree
- TR Denotes deciduous tree

DESCRIPTION OF PROPERTY SURVEYED

(Per Commercial Partners Title, a division of Chicago Title Insurance Company, Commitment for Title Insurance File No. CP72041, Supplemental No. 1, commitment date June 30, 2023)

Lots 31, 32, 33 and 34, Auditor's Subdivision No. 340.

Hennepin County, Minnesota
 Torrens Property

PLAT RECORDING INFORMATION

AUDITOR'S SUBDIVISION NUMBER 340 was filed of record on September 12, 1946 in Book 116 of Plots, Page 33.

TITLE COMMITMENT

Commercial Partners Title, a division of Chicago Title Insurance Company, Commitment for Title Insurance File No. CP72041, Supplemental No. 1, commitment date June 30, 2023, was relied upon as to matters of record.

Schedule B Exceptions:

Exceptions are indicated on survey with circled numbers unless otherwise noted. Items not listed below are standard exceptions and/or are not survey related.

9.) Terms and conditions of and temporary easement for construction and related purposes in favor of the Metropolitan Council, a public corporation and political subdivision of the State of Minnesota being described as Parcel No. 105, contained in Temporary Construction Easement dated April 27, 2021, filed May 6, 2021, as Document No. 5833598. (Lot 31) [Shown on survey]

10.) Terms and conditions of and temporary easement for construction and related purposes in favor of the Metropolitan Council, a public corporation and political subdivision of the State of Minnesota being described as Parcel No. 106, contained in Temporary Construction Easement dated April 27, 2021, filed May 6, 2021, as Document No. 5833611. (Lot 32) [Shown on survey]

11.) Terms and conditions of and temporary easement for construction and related purposes in favor of the Metropolitan Council, a public corporation and political subdivision of the State of Minnesota being described as Parcel No. 107, contained in Temporary Construction Easement dated April 27, 2021, filed May 6, 2021, as Document No. 5833675. (Lot 33) [Shown on survey]

12.) Terms and conditions of and temporary easement for construction and related purposes in favor of the Metropolitan Council, a public corporation and political subdivision of the State of Minnesota being described as Parcel No. 108, contained in Temporary Construction Easement dated April 27, 2021, filed May 6, 2021, as Document No. 5833612. (Lot 34) [Shown on survey]

OWNER:
 HRA City of Richfield
 6700 Portland Ave S.
 Richfield, MN 55423

APPLICANT:
 Beacon Interfaith Housing Collaborative
 2610 University Avenue West
 Suite 100
 St. Paul, MN 55114
 Contact: Jim Barnes
 jrbarnes@beaconinterfaith.org
 651-377-8183

I hereby certify that this survey, plan, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Dated this 18th day of July, 2023

SUNDE LAND SURVEYING, LLC
 By: *Leonard F. Carlson*
 Leonard F. Carlson, P.L.S. Minn. Lic. No. 44890

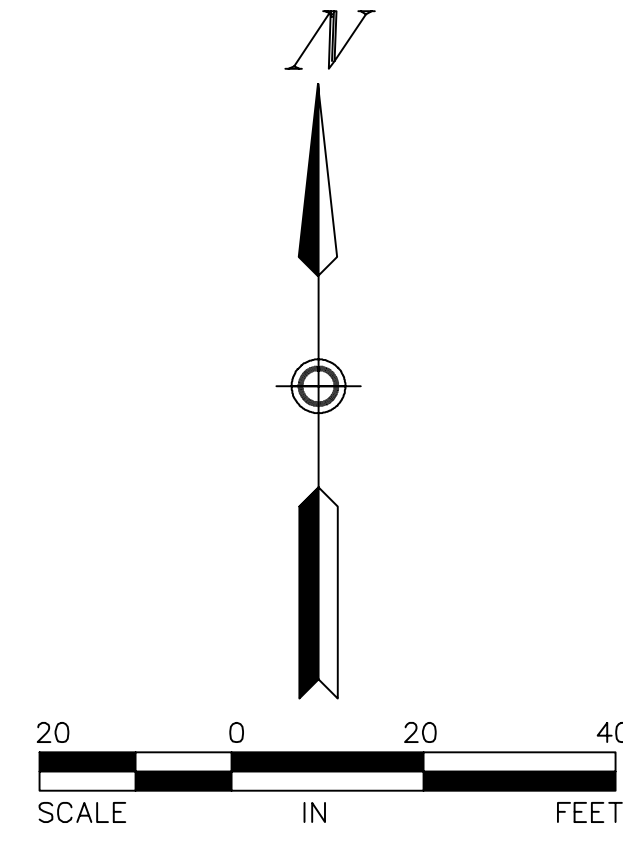
revised right of way line per comments	NDN	9/18/23
Revision	By	Date
	NDN	

Drawing Title:
**PRELIMINARY PLAT OF:
 ASTER COMMONS**

SUNDE LAND SURVEYING
 www.sunde.com

Main Office:
 9001 East Bloomington Freeway (35W) • Suite 118
 Bloomington, Minnesota 55420-3435
 952-881-2455 (Fax: 952-888-9526)

Project: 2015-003-A	Blk/Pg: 1094/39	Date:
Township: 028	Range: 24	Section: 26
File: 2015003A_preplat.dwg		Sheet: 1 of 1



Project No. 2022018

Aster Commons
 6613 Portland Avenue South
 Richfield, MN 55423

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Architect Seal

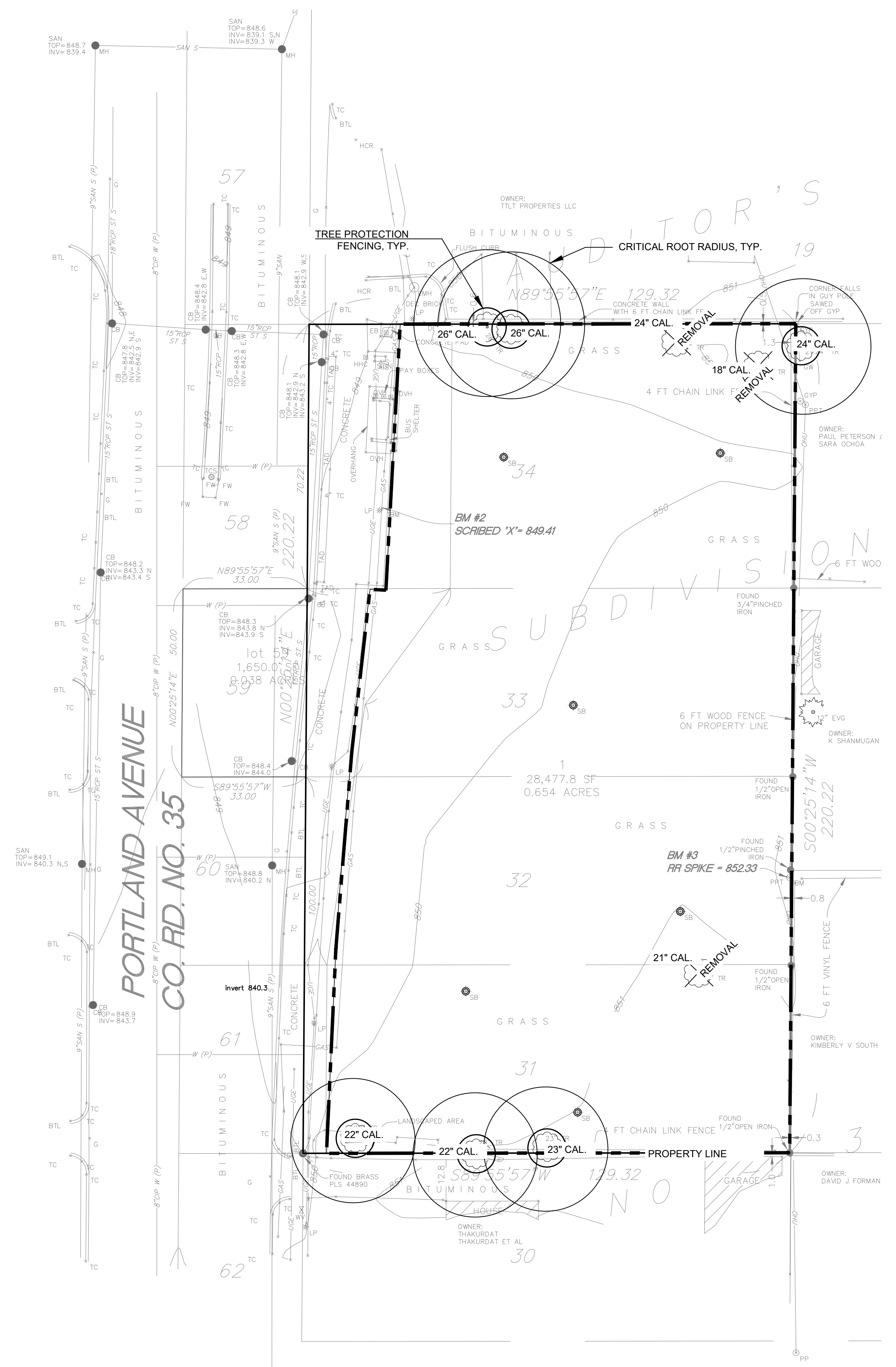
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 Print Name _____
 Date _____ License No _____

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Mark	Description

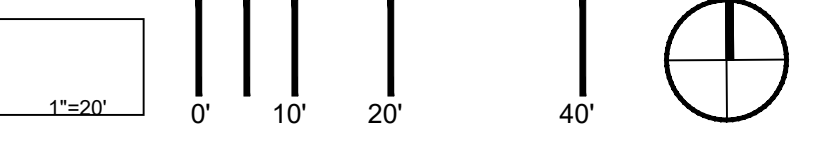
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TREE PROTECTION PLAN

LA200



1 L-200 TREE PROTECTION PLAN



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Richfield, MN 55423

Project No. 2022019

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Architect Seal

Signature _____
Print Name _____
Date _____ License No. _____

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Mark	Description

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FENCING PLAN

LA300

Trex Seclusions® Composite Fencing

SRE Fence & Supply Co.
Trex Fence Distribution
Trex Company National Distribution Partner

ARCHITECTURAL DRAWING:
TREX SECLUSIONS FENCING
PROFILE VIEW

BILL OF MATERIAL FOR A 6'H x 8'W SECTION OF SECLUSIONS FENCINGS		
C NTS	QUANTITY	LENGTH
Post Cap: Pyramid, Flat	1	
5" x 5" Post	1	106"*
4" x 4.9" Top Rail	1	91"*
1"x5.75" Interlocking Picket	19	67"*
1" x 5.75" Bottom Rail Cover	2	91"*
Aluminum Bottom Rail	1	90 1/2"
Fence Bracket	4	
1 5/8" (Typ) Exterior Wood Screws	24	

* Length may vary

NOTES:

- INSTALLATION TO BE COMPLETED PER MANUFACTURER'S SPECIFICATION.
- THIS DRAWING IS PROVIDED FOR PLANNING PURPOSES. REFER TO MANUFACTURER'S INSTALLATIONS FOR CONSTRUCTION DETAILS.
- REFER TO MANUFACTURER'S WEBSITE FOR PRODUCT INFORMATION.
- DRAWING NOT TO SCALE.

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SRE Fence & Supply Co.
1745 Shea Center Dr., Ste. 378
Highlands Ranch, CO 80129

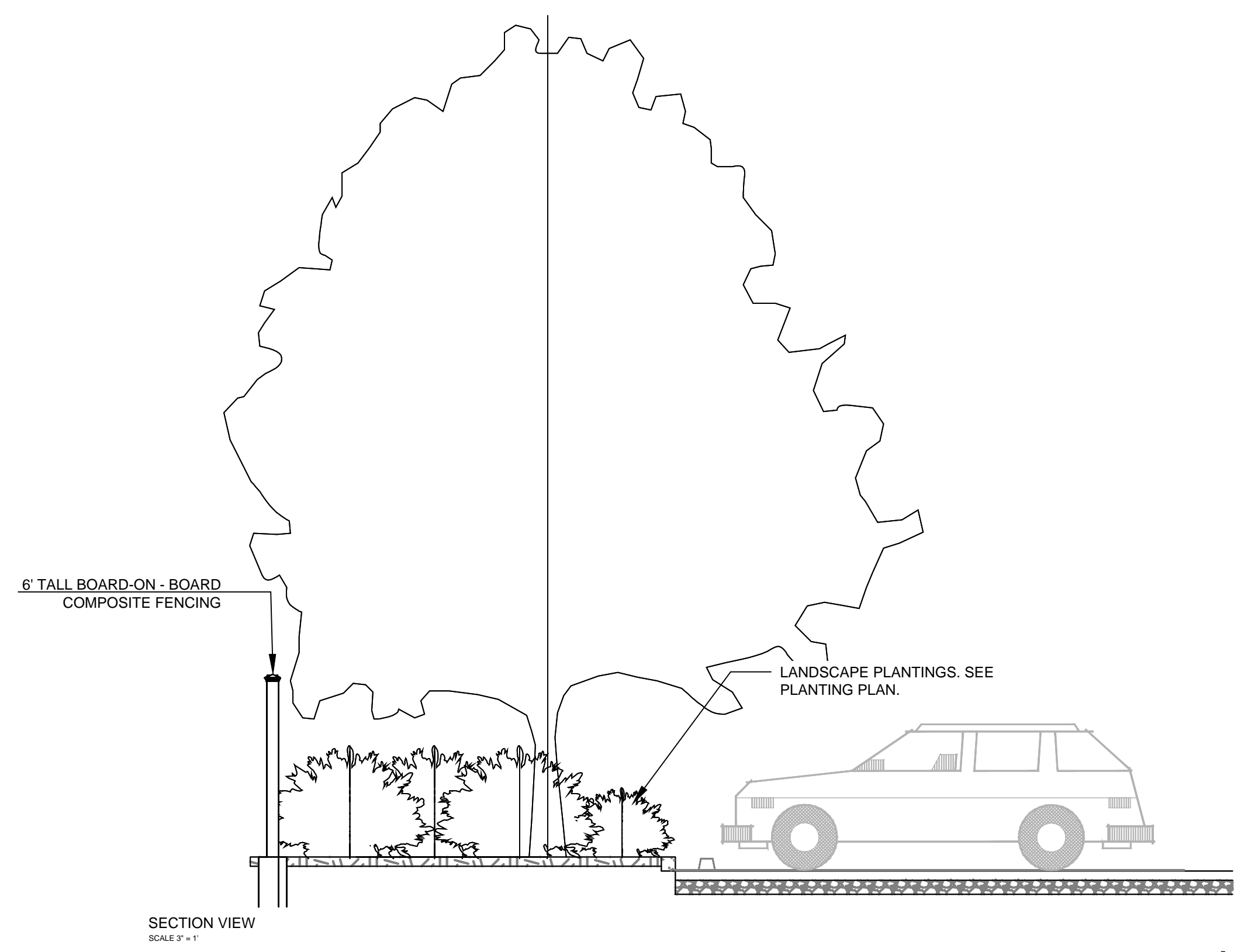
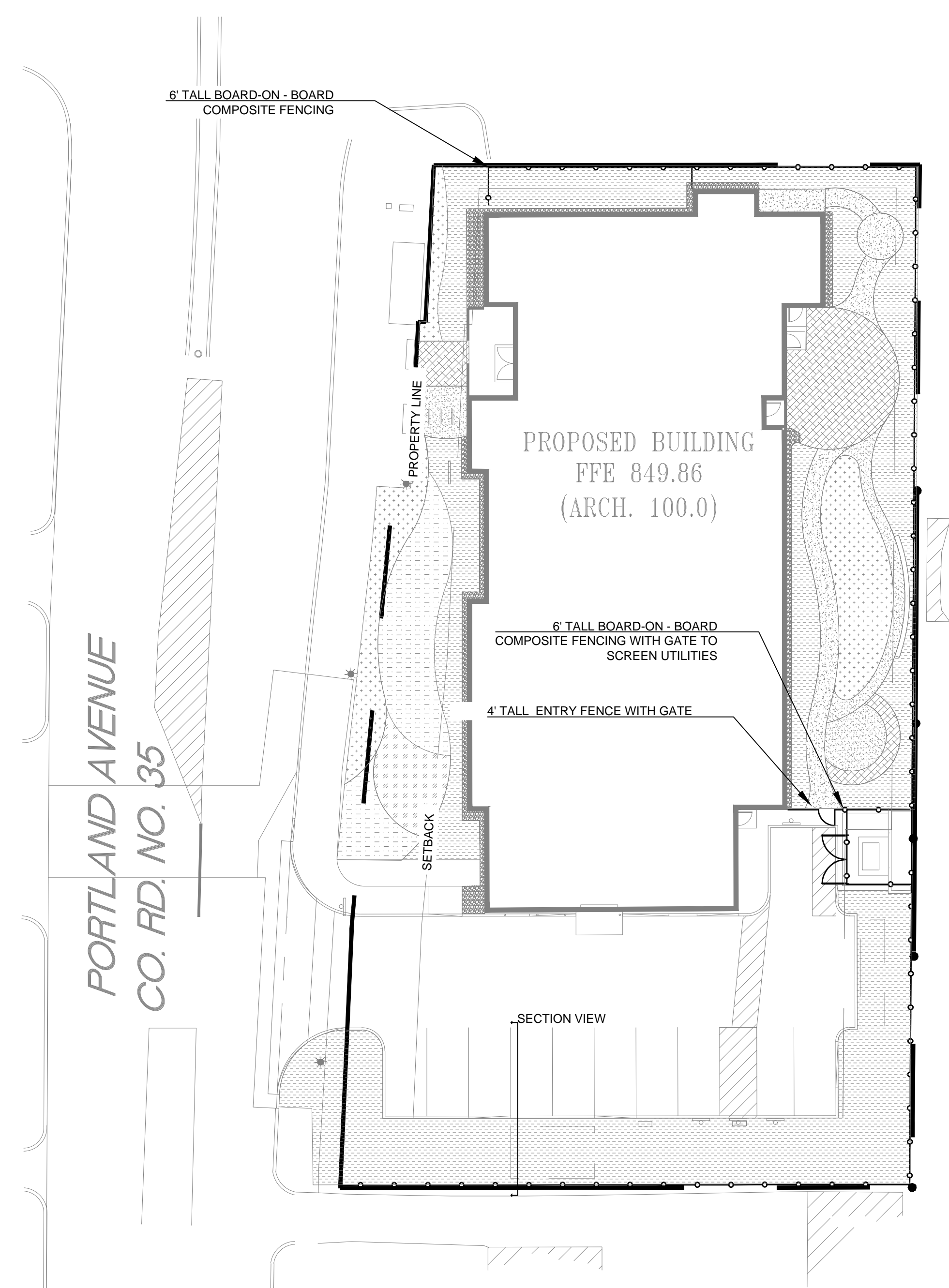
SITE MATERIAL LEGEND

- CONCRETE
- PERMEABLE PAVER
- STABILIZED GRANITE WALKWAY (PERMEABLE)
- 3/4"-1" ROCK MULCH
- HARDWOOD MULCH WITH PLANTINGS - SEE PLANTING PLAN
- RAINGARDEN : ZONE 1
- RAINGARDEN : ZONE 2
- POLLINATOR LAWN SEED MIX

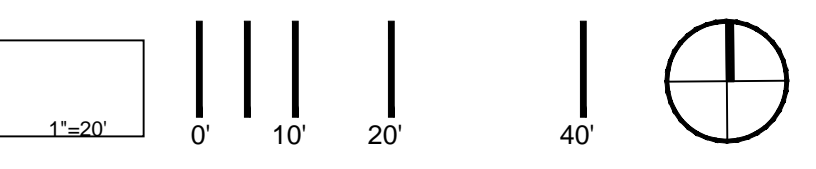
FENCING GENERAL NOTES

- THERE ARE THREE FENCING UNITS IDENTIFIED IN THE PROPOSED SITE:
- 448 LINEAR FEET OF 6' TALL BOARD-ON-BOARD COMPOSITE FENCING SURROUNDS THE SIDE AND BACK YARDS OF THE SITE. FENCING IS SET-BACK 6' INTO SITE FROM PROPERTY LINE.
 - 132 LINEAR FEET OF 6' TALL BOARD-ON-BOARD COMPOSITE FENCING WITH ACCESS GATE SURROUNDS THE TRANSFORMER AREA.
 - 12 LINEAR FEET OF 4' RAIL FENCE WITH ENTRY GATE AT THE ENTRY TO THE RESIDENTIAL BACKYARD.
 - FENCING TYPE WAS CHOSEN FOR ITS RESIDENTIAL CHARACTER.

PRODUCT FINISH AND DESIGN



SECTION VIEW
SCALE 3/4" = 1'



PLANT SCHEDULE

DECIDUOUS TREES	QTY	COMMON / BOTANICAL NAME
Ex	6	Existing Tree To Remain /
Aa4	3	Autumn Blaze Maple / <i>Acer x freemanii</i> 'Jeffersd' TM
Ag2	3	Autumn Brilliance Serviceberry / <i>Amelanchier x grandiflora</i> 'Autumn Brilliance'
Br2	1	River Birch / <i>Betula nigra</i> Multi Trunk, 2 Trunk Minimum
PB	8	Pillar Birch / <i>Betula platyphylla</i> Single Trunk
Bw	3	Whitespire Birch / <i>Betula populifolia</i> 'Whitespire'
Cw	8	Golden Shadows Dogwood / <i>Cornus alternifolia</i> Wstackman
As	4	Ware's Oak 'Kindred Spirit' / <i>Quercus x warei</i> 'Nadler'
Ts	2	American Linden / <i>Tilia americana</i> 'Sentry'

EVERGREEN TREES	QTY	COMMON / BOTANICAL NAME
Uo	2	Discovery Elm / <i>Ulmus davidiana</i> var. <i>japonica</i>
Je	5	Eastern Red Cedar / <i>Juniperus virginiana</i>

SHRUBS	QTY	COMMON / BOTANICAL NAME
Ai2	88	Iroquois Beauty Black Chokeberry / <i>Aronia melanocarpa</i> 'Iroquois Beauty' TM
Cs2	15	Cardinal Red Twig Dogwood / <i>Cornus sericea</i> 'Cardinal'
SC	93	Sandcherry / <i>Prunus pumila</i>

PERENNIALS	QTY	COMMON / BOTANICAL NAME
Ac	125	Lady's Mantle / <i>Alchemilla mollis</i>
Ag	68	Goatsbeard / <i>Aranunc dioicus</i>
Af	22	False Spiraea / <i>Astilbe japonica</i> 'Deutschland'
Nw	5	Celastrus Scandens 'Bailum' / Autumn Revolution Bittersweet
Cb	134	Bicknell's Sedge / <i>Carex bicknellii</i>
Co	34	Blue Sedge / <i>Carex glauca</i>
Cos	117	Carex bicknellii / Copper Shouldered Oval Sedge

PLANNING REQUIREMENTS

THE PROPOSED BUILDING CONTAINS 38 UNITS. PER PLANNING REQUIREMENTS, 5 TREES ARE REQUIRED FOR EACH UNIT. THIS PROJECT REQUIRES AN ADDITIONAL 19 TREES. THIS PLAN PROPOSES 39 TREES BE ADDED TO THE SITE IN A RANGE OF SIZES, SPECIES, AND VARIETIES. 20% OF THE SPECIES ARE TO BE PLANTED WITH A CALIPER OF 4.5", 30% WITH A CALIPER OF 30%, AND 50% WITH A CALIPER OF 2.5".

TOTAL SITE AREA: 25336 SQ FEET
TOTAL LANDSCAPE AREA: 6378 SQ FEET (25% OF TOTAL SITE)

ALL PLANTINGS WERE SELECTED TO MAXIMIZE ECOLOGICAL STATUS. ALL PLANTINGS EITHER ARE NATIVE OR ARE KNOWN TO HAVE HIGH ENVIRONMENTAL PERFORMANCE (POLLUTION CAPTURE OR HEAT TOLERANCE).

SITE MATERIAL LEGEND

- CONCRETE
- PERMEABLE PAVER
- STABILIZED GRANITE WALKWAY (PERMEABLE)
- 3/4"-1" ROCK MULCH
- HARDWOOD MULCH WITH PLANTINGS - SEE PLANTING PLAN
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- POLLINATOR LAWN SEED MIX

Architecture and Interiors
510 Marquette Avenue South, Suite 200
Minneapolis, MN 55402 | 612.375.0336

Aster Commons
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Richfield, MN 55423

Project No. 2022018

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Architect Seal

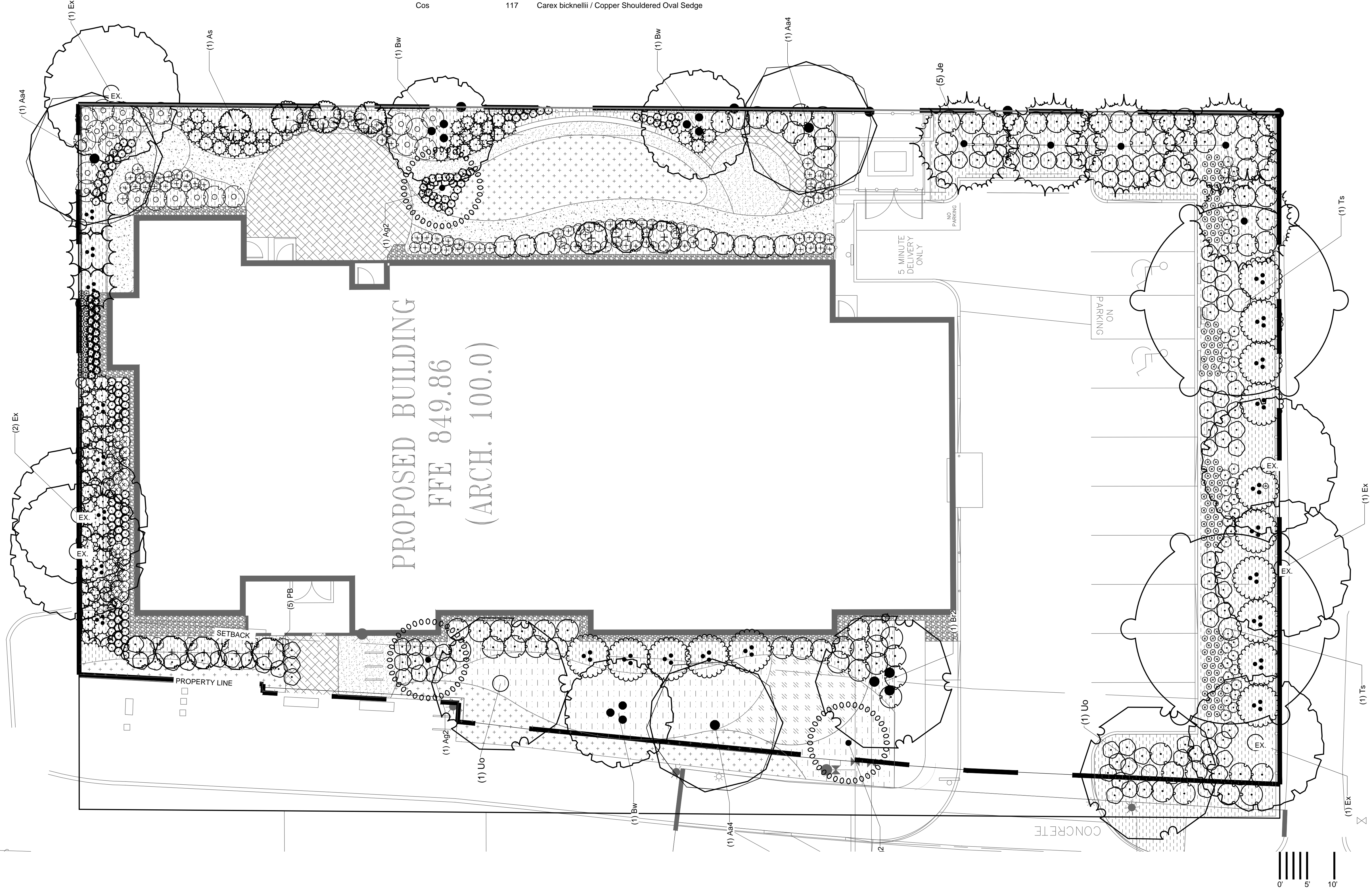
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PLANTING PLAN

LA500



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